



DEPARTMENT OF CITY PLANNING

RECOMMENDATION REPORT

City Planning Commission

Date: June 23, 2022
Time: After 8:30 a.m.
Place: In conformity with the Governor's Executive Order N-29-20 (March 17, 2020) and due to concerns over COVID-19, the CPC meeting will be conducted entirely telephonically by Zoom [<https://zoom.us/>]. The meeting's telephone number and access code access number will be provided no later than 72 hours before the meeting on the meeting agenda published at <https://planning.lacity.org/about/commission/sboards-hearings> and/or by contacting cpc@lacity.org

Public Hearing: March 16, 2022
Appeal Status: General Plan Amendment is not appealable. Vesting Zone Change and Height District Change are appealable by the Applicant to the City Council if disapproved in whole or in part. All other actions are appealable to City Council.

Expiration Date: July 14, 2022

Multiple Approval: Yes

PROJECT

LOCATION: 650-676 South San Vicente Boulevard

PROPOSED PROJECT:

The Project proposes 140,305 square feet of medical office space, 4,000 square feet of restaurant, and 1,000 square feet for retail, such as a pharmacy, on a 0.74-net acre site. The Project Site is currently improved with two buildings and associated surface parking, which would be demolished. The proposed uses would be built within a 12-story, building that includes ground floor lobby and commercial space, four levels of above-ground podium parking, and seven levels of medical office uses.

REQUESTED ACTIONS: ENV-2017-468-EIR

1. Pursuant to Section 21082.1(c)(3) of the California Public Resources Code (PRC), the consideration and certification of the Environmental Impact Report (EIR), ENV-2017-468-EIR, SCH No. 2020010172, for the above-referenced project, and adoption of the Statement of Overriding Considerations setting forth the reason and benefits of adopting the EIR with full knowledge that significant impacts may remain.
2. Pursuant to Section 21081.6 of the California PRC, the adoption of the proposed Mitigation Measures and Mitigation Monitoring Program.

Case No.: CPC-2017-467-GPA-VZC-HD-SPR
CEQA No.: ENV-2017-468-EIR
Related Case: VTT-74865, -1A
Council No.: 5 – Koretz
Plan Area: Wilshire
Plan Overlay: Oil Drilling District
Certified NC: Mid City West
Existing GPLU: Limited Commercial
Proposed GPLU: Regional Commercial
Existing Zone: C1-1VL-O
Proposed Zone: (T)(Q)C2-2D-O

Applicant: 650-676 SSV Property Owner, LLC and 650 SSV Property Owner, LLC

Representative: Sheri Bonstelle, JMBM LLP

3. Pursuant to Section 21081 of the California PRC, the adoption of the required Findings for the certification of the EIR.

CPC-2017-467-GPA-VZC-HD-SPR

1. Pursuant to **Los Angeles Municipal Code (LAMC) Section 11.5.6 and City Charter Section 555**, a **General Plan Amendment** to the Wilshire Community Plan to change the land use designation from Limited Commercial to Regional Commercial; and
2. Pursuant to **LAMC Section 12.32 P and Q**, a **Vesting Zone Change and Height District Change**, from C1-1VL-O to (T)(Q)C2-2D-O to allow for a FAR of up to 4.5:1 and up to a 20 percent reduction in vehicle parking; and
3. Pursuant to **LAMC Section 16.05**, a **Site Plan Review** for a development that results in an increase of over 50,000 square feet of nonresidential floor area.

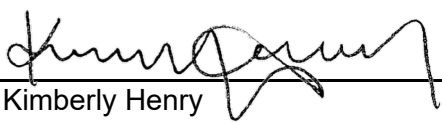
RECOMMENDED ACTIONS:

If the City Planning Commission denies the appeal of the Vesting Tentative Tract Map (VTT-74865-1A) and sustains the actions of the Advisory Agency:

1. **Find**, based on the independent judgment of the decision-maker, after consideration of the whole of the administrative record, the project was assessed in the 656 South San Vicente Medical Office Project EIR, which includes the Draft EIR, ENV-2017-468-EIR (State Clearinghouse House No. 2020010172), dated June 2021, the Final EIR, dated January 2022, and the Erratum dated February 2022, that will have been considered by the City Planning Commission prior to this case; and pursuant to CEQA Guidelines, Sections 15162 and 15164, no subsequent EIR or addendum is required for approval of the project;
2. **Recommend** that the Mayor and City Council approve a **General Plan Amendment** to the Wilshire Community Plan to change the land use designation from Limited Commercial to Regional Commercial;
3. **Recommend** that the City Council approve a **Vesting Zone Change and Height District Change**, from C1-1VL-O to (T)(Q)C2-2D-O to allow for a FAR of up to 4.5:1 and up to a 20 percent reduction in vehicle parking;
4. **Approve a Site Plan Review** for a development that results in an increase of over 50,000 square feet of non-residential floor area;
5. **Adopt** the attached Conditions of Approval; and
6. **Adopt** the attached Findings.

VINCENT P. BERTONI, AICP
Director of Planning


Milena Zasadzien
Senior City Planner


Kimberly Henry
City Planner


Paul Caporaso
Planning Assistant

ADVICE TO PUBLIC: *The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Written communications may be mailed to the Commission Secretariat, Room 272, City Hall, 200 North Spring Street, Los Angeles, CA 90012 (Phone No. 213-978-1300). While all written communications are given to the Commission for consideration, the initial packets are sent to the week prior to the Commission's meeting date. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendaized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters, assistive listening devices, or other auxiliary aids and/or other services may be provided upon request. To ensure availability of services, please make your request not later than three working days (72 hours) prior to the meeting by calling the Commission Secretariat at (213) 978-1300.

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A1 – Project Renderings	
B – Mitigation Monitoring Program, dated June 2022	
C – Supplemental Parking Analysis	
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Environmental Impact Report (EIR) links:	
Draft EIR:	
https://planning.lacity.org/development-services/eir/656-south-san-vicente-medical-office-project-0	
Final EIR:	
https://planning.lacity.org/development-services/eir/656-south-san-vicente-medical-office-project-1	
Erratum:	
https://planning.lacity.org/eir/656_SoSanVicenteMedicalOffice/Errata.pdf	

PROJECT ANALYSIS

PROJECT SUMMARY

The 656 South San Vicente Medical Office Project (Project) includes the demolition of two existing buildings and associated surface parking lots, and the construction of a mixed-use medical office building with up to 145,305 square feet of new floor area on an approximately one-acre site. The Project Site is currently improved with two buildings, comprised of a 5,738 square-foot, vacant educational building, and an 8,225 square foot Big 5 Sporting Goods store, combined totaling 13,963 square feet of existing floor area. The Project proposes 140,305 square feet of medical office space, 4,000 square feet of restaurant space, and 1,000 square feet for other commercial uses, such as a pharmacy. The proposed uses would be built within a single, 12-story building that includes ground floor lobby and commercial space, four levels of podium parking, and seven levels of medical office uses. In response to community concerns regarding localized health impacts, construction impacts, shade and shadow impacts, and traffic congestion issues, the project has implemented mitigation measures to minimize construction emissions and noise impacts, designed the tower to “step back” from the adjacent residential neighborhood as it increases in stories, and has provided adequate vehicular access points, operational standards, and parking spaces to minimize potential traffic and other vehicular conflicts. The Project would be compatible with the pattern of development in the area and would support City policies relating to the critical need for contemporary medical office and lab facilities and transit-oriented development.

BACKGROUND

Location and Setting

The trapezoid-shaped Project Site is located within the Wilshire Community Plan area of the City of Los Angeles, in the Beverly Grove neighborhood, adjacent to the City of Beverly Hills. The Project Site is bounded by Orange Street to the north, a shared alleyway to the northeast, Sweetzer Avenue to the east, and South San Vicente Boulevard to the southwest. Primary regional automobile access is provided by Santa Monica Freeway (I-10) and the Hollywood Freeway (US-101), which are accessible within roughly two and four miles, respectively, from of the Project Site. Major arterials providing regional access to the Project Site include South San Vicente Boulevard and Wilshire Boulevard. The Project Site is located within a Transit Priority Area (ZI-2452) and is approximately 0.25 miles east of the future Metro D (Purple) Line Wilshire/La Cienega station.

Project Site Characteristics

The topography of the Project Site is relatively flat throughout the entirety of the site. The trapezoid-shaped site has approximately 361 feet of frontage along the South San Vicente Boulevard frontage Road, 235 feet of frontage along a shared alley, 138 feet of frontage along Sweetzer Avenue, and 109 feet of frontage along Orange Street.

The Project Site is located within a Liquefaction Zone, a Methane Zone, and an Oil Drilling District, although no oil extraction or production occurs on the site. The site is not located within a Very High Fire Hazard Severity Zone, Flood Zone, or the Alquist-Priolo Fault Zone. No other special use districts are applicable to the Project Site.

Aerial View



Existing Development

The Project Site is presently developed with a 5,738 square-foot vacant educational building and an 8,225 square-foot Big 5 Sporting Goods store.¹ Surface parking associated with these uses is located on the southeastern portion of the Project Site, abutting the frontage road of South San Vicente Boulevard, South Sweetzer Avenue, and the alley to the northeast. Additional surface parking is in the middle of the Project Site between the two buildings, and to the rear of the Big 5 Sporting Goods store on the southern portion of the site. The parking areas are accessible from two driveways located along the frontage road of South San Vicente Boulevard and two driveways located along the alleyway.

Existing Land Use and Zoning

The Wilshire Community Plan designates the Project Site for Limited Commercial land uses. The site is zoned C1-1VL-O (Commercial Zone, Height District 1 Very Limited, Oil Drilling District). The C1 Zone allows for retail stores up to 100,000 square feet of floor area, offices, businesses, hotels, hospitals/clinics, museums, and multi-family residential uses. No setbacks are required for front yards, side yards, and rear yards for commercial uses in the C1 Zone. Height District 1VL imposes a 45-foot height limit for commercial uses and permits up to three stories in height, and allows for a Floor Area Ratio (FAR) of 1.5:1. Although the Project Site is

¹ The 5,738 square-foot vacant building previously housed the Montessori Children's World School. As the building was vacated October 2018, credit for this use was included as part of the baseline under CEQA as this reflects the amount of floor area that was in active use during those previous years.

within a City-designated Oil Drilling District, that designation merely identifies districts where the drilling of oil wells or the production from the wells of oil, gases or other hydrocarbon substances is permitted (it does not apply to subterranean gas holding areas which are operated as a public utility). Based on Phase I Environmental Site Assessments (ESAs) a historic buried and idle oil well had been located on the northeastern portion of the Project Site; however, the well never produced any oil and there is no evidence of oil production activities on the Project Site. As such, the identified well does not represent a recognized environmental condition in connection with the Project.

Surrounding Land Uses

The Project Site is located in an urbanized area and surrounded by a mix of land uses that include multi-family residential and commercial uses, ranging from low-rise to high-rise buildings, which are physically separated from the Project Site by streets and an alley, as follows:

North: To the north of the Project Site across Orange Street are multi-story office buildings and are designated for Limited Commercial land uses and are entirely within the CR-1L-O Zone.

Northeast: To the northeast of the Project Site across the alleyway, as well as beyond Orange Street are two-story multifamily residential uses. These properties are designated for both Low Medium I and Medium Residential land uses and are within the R3-1-O and R2-1-O Zones.

East: To the east of the Project Site across Sweetzer Avenue are multi-story commercial and office uses. These properties are designated for Regional Commercial land uses within the [Q]C4-2D-O zone.

South: To the south and southeast of the Project Site across Wilshire Boulevard and Sweetzer Avenue are multi-story commercial and office uses. These properties are designated for Regional Commercial land uses within the [Q]C4-2D-O and C2-2D-O Zones.

West: To the west of the Project Site across South San Vicente Boulevard are multi-story office uses and one-story commercial uses with surface parking, located entirely within the City of Beverly Hills.

Streets and Circulation

South San Vicente Boulevard is designated by the Mobility Plan as a Boulevard II and dedicated to a right-of-way width of 110 feet along the Project Site frontage. South San Vicente Boulevard is improved with a frontage road, sidewalks, curbs, and gutters. The Bureau of Engineering has not made any dedication requests at this time.

Orange Street is designated by the Mobility Plan as a Local Street - Standard and dedicated to a right-of-way width of 60 feet along the Project Site frontage. Orange Street is improved with sidewalks, curbs, and gutters. The Bureau of Engineering has requested a 2.5-foot wide strip of land be dedicated along Orange Street to complete a 30-foot half right-of-way in accordance with Local Street standards, including a 15-foot by 15-foot property line cut corner or 20-foot radius property line return at the intersection with San Vicente Boulevard.

Sweetzer Avenue is designated by the Mobility Plan as a Collector Street and dedicated to a right-of-way width of 66 feet along the Project Site frontage. Sweetzer Avenue is improved with sidewalks, curbs, and gutters. The Bureau of Engineering has requested a 3-foot wide strip of land be dedicated along Sweetzer Avenue to complete a 33-foot half right-of-way in accordance

with Collector Street standards, including a 15-foot by 15-foot property line cut corner or 20-foot radius property line return at the intersection with San Vicente Boulevard.

Alleyway is a 20-foot-wide alleyway located along the Project Site's northeast frontage, between the Project Site and two adjacent multi-family residential buildings. The Alleyway is improved with a center v-gutter. The Bureau of Engineering has not made any dedication requests at this time.

Public Transit

The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within .25 miles of the Project Site. These rapid bus and local bus service lines include Metro Lines 720, 20, and 105.

Land Use Policies

The Project Site is located within the Wilshire Community Plan area and is not within any specific plans or supplemental use districts, except for an Oil Drilling District, as described above under "Existing Land Use and Zoning", although no oil extraction or production occurs on the site.

General Plan Framework

The City of Los Angeles General Plan Framework Long Range Land Use Diagram generally identifies the properties adjacent to the Project Site to the east along the Wilshire Boulevard corridor for Regional Commercial land uses. Regional Centers are considered a focal point of regional commerce, identity and activity and contain a diversity of uses such as corporate and professional offices, residential, retail commercial malls, government buildings, major health facilities, major entertainment and cultural facilities and supporting services. Generally, different types of Regional Commercial uses will fall within the range of floor area ratios from 1.5:1 to 6:1 and are characterized by developments six- to 20-stories (or higher) in height.

Wilshire Community Plan

The Wilshire Community Plan designates the Project Site for Limited Commercial land uses. According to the Community Plan, the corresponding zones for Limited Commercial designation are C1, CR, C1.5, P, RAS3, RAS4. The existing C1 zoning is consistent with this designation.

Site Related Cases and Permits

On-Site Cases

Case No. VTT-74865: In conjunction with the proposed Project, on March 10, 2022, the Deputy Advisory Agency approved a Vesting Tentative Tract Map for the merger of the seven ground lots into a single ground lot, with a site area of a 0.74 net acre (32,290 square-feet) and a Haul Route for the export of up to 12,222 cubic yards of soil. This decision was subsequently appealed by three aggrieved parties and is pending decision by the City Planning Commission concurrent with the subject case.

Off-Site Cases

No recent related off-site cases were found within 500 feet of the Project Site.

PROJECT DETAILS

The Project would demolish a 5,738 square-foot, vacant educational building, and an 8,225 square-foot Big 5 Sporting Goods store and associated surface parking to develop a medical office and retail-commercial development on an approximately 0.74-net acre (33,060 gross square-foot, 32,290 net square-foot) site located at 650–676 South San Vicente Boulevard (Project Site). The Project would include up to 145,305 square feet of floor area, comprised of 140,305 square feet of medical office space and 5,000 square feet of ground floor retail-commercial space, of which up to 4,000 square feet may be a restaurant and 1,000 square feet may be other retail commercial uses, such as a pharmacy. The proposed building would be 12 stories and would measure approximately 218 feet in height (230 feet to the top of the mechanical penthouse). The Project would include seven floors of medical office uses over four floors of above-grade parking, and a ground floor containing a lobby for the medical office, and commercial uses fronting South San Vicente Boulevard. The Project would provide full-valet services for 418 parking spaces, including 393 vehicle parking spaces for medical office uses and 25 vehicle parking spaces for retail-commercial uses. The Project would also provide full-valet service for bicycle parking and would include 716 bicycle parking spaces for short- and long-term use. The building is oriented to the trapezoidal shape of the Project Site and the Project's primary frontage along South San Vicente Boulevard. The building footprint would occupy the entirety of the Project Site, and would be designed in a contemporary architectural style, in a similar fashion to recent commercial and medical office development in the vicinity and along Wilshire Boulevard.

Building Design

Street Perspective looking northwest from South San Vicente Boulevard and Wilshire Boulevard



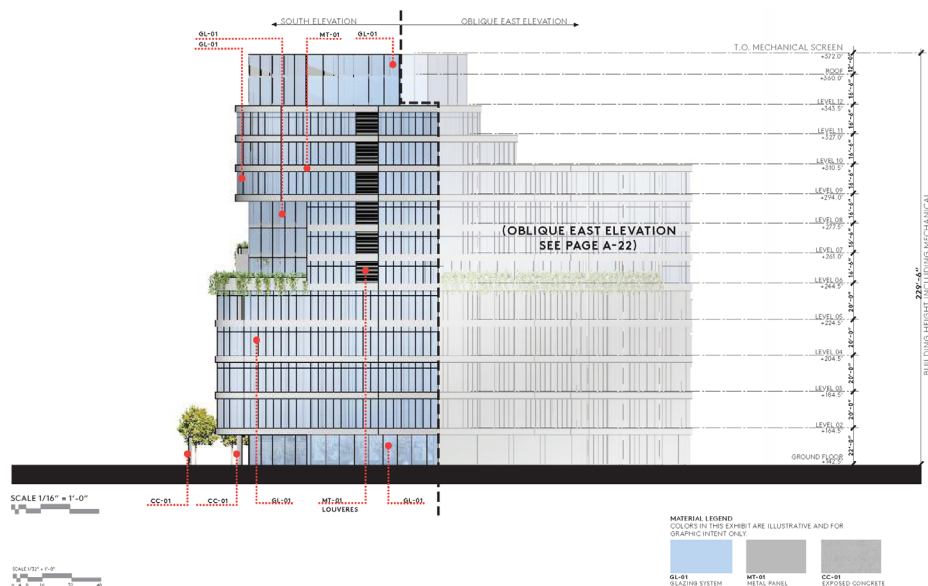
Aerial Perspective looking northwest from South San Vicente Boulevard and Wilshire Boulevard



Due to the trapezoidal shape of the Project Site and the angle at which South San Vicente and Wilshire Boulevard intersect (roughly 100 degrees) the elevation along South Sweetzer Avenue faces southeast, and is referred to as the “South Elevation”, and the primary elevation along San Vicente Boulevard facing southwest is referred to as the “West Elevation”. While the South Sweetzer elevation does not serve as the primary frontage for the Project, it does provide a ground floor entrance to the proposed 4,000 square-foot restaurant space that is accessible from the outdoor dining plaza.

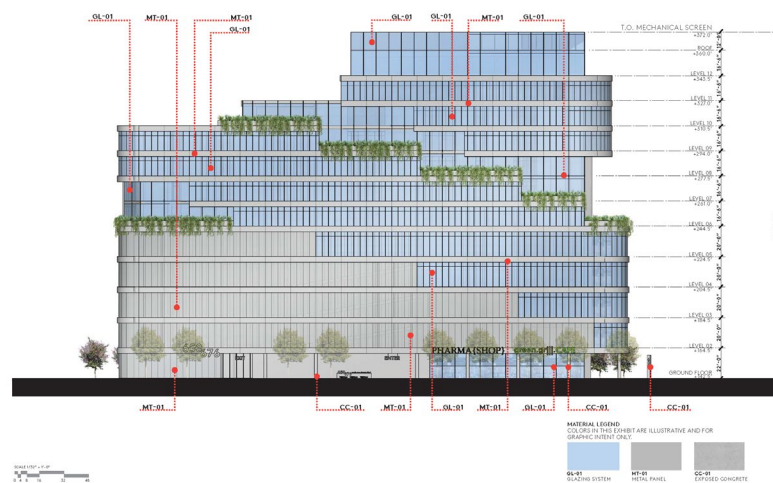
The restaurant entrance is distinct in its orientation towards the outdoor dining plaza, fronting the intersection of South San Vicente Boulevard, South Sweetzer Avenue, and Wilshire Boulevard. As shown in the figure below, the materials used in the South Elevation include glazed glass panels, grey metal paneling, and exposed concrete columns. These materials are the same as those incorporated across all facades of the Project. The South Elevation demonstrates variation in the vertical façade through a slight façade set back and inset balcony space that occupies a portion of roughly three stories.

South Elevation



The West Elevation runs parallel to South San Vicente and serves as the front yard and primary frontage to the Project. The façade of the West Elevation includes the primary bike and vehicular/valet driveway as well as the pedestrian entrance to the medical office lobby. Like the other elevations, the West Elevation includes glazed glass panels, grey metal paneling, and exposed concrete columns, utilizing a more even mix of metal paneling and glazed glass panels, as shown in the figure below, which primarily serves to screen the four-level above-ground parking garage. The most distinctive architectural feature of the West Elevation is the terraced balconies that step up vertically from the southern to northern ends of the façade, in addition to a separate, two-story balcony on the northern end of the façade.

West Elevation

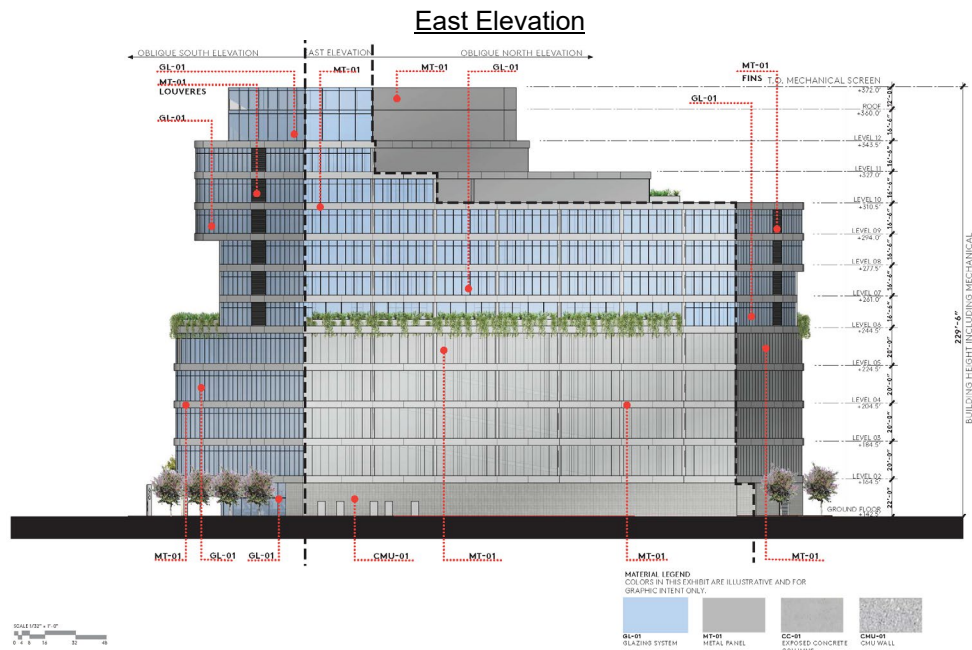


The North Elevation, fronting Orange Street, is similar to the South Elevation in terms of massing and scale, but uses less glazed glass paneling, and provides no immediate pedestrian entrances. Despite the limited pedestrian access, the North Elevation includes an employee vehicular/bike valet entrance as all the generator and loading areas front Orange Street, which helps to further define the North Elevation of the Project as the “back of house”. This design is intentional, as the North Elevation is directly across from existing commercial development, rather than multi-family housing, as is the case with the alleyway, which is discussed further below. The positioning of the loading areas and driveway egress are approved by the Los Angeles Department of Transportation, per their internal standards and guidelines.

North Elevation



The East Elevation, shown in the image below, abuts the existing low-rise multi-family developments, and serves as the eastern frontage along the alleyway. As such, the East Elevation does not have any entrances nor loading areas. While the façade incorporates the building's architectural elements and materials, the East Elevation is primarily comprised of grey metal paneling which screens the four-story above-ground parking podium, glazed glass paneling from Level 5 upward, and a landscaped ledge that serves as a visual continuation of the balcony spaces of the South and Western Elevations.



Open Space and Landscaping

As the Project is a non-residential medical office and commercial Project, LAMC Section 12.21 G open space requirements do not apply. However, the Project is providing the following landscaping and open space amenities as part of the Project. The ground level streetscape includes landscaping and seating for the potential outdoor dining area that would be located on the corner of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible area from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area. In addition, outdoor landscaped terraces would be provided on Floors 6 through 10 of the building along San Vicente Boulevard, ranging in size from 234 square feet to 1,465 square feet in area.

There are seven trees existing on the Project Site, all of which are significant (eight inches or greater of cumulative trunk diameter if multi-trunked, as measured 54 inches above ground), non-protected trees. The Project would replace all significant, non-protected trees at 1:1 ratio with a minimum of 24-inch box tree. The Project would provide 17 trees on the ground level, with additional trees provided in the small terraced landscaped patios on Floors 6 through 10.

Parking, Loading, and Access

Vehicle and Bicycle Parking

Pursuant to LAMC 12.21 A.4(c), the combination of medical office and retail-restaurant uses would require a total of 746 vehicle parking spaces. Pursuant to LAMC Section 12.32 P, the Project is requesting a reduction in parking not to exceed 20 percent for a commercial project incident to the Project's legislative actions, reducing the required vehicle parking to a total 597 spaces. Pursuant to LAMC Section 12.21 A.16(a)(2) the Project is required to provide short-term and long-term parking spaces for all commercial uses, for a total of 58 bicycle parking spaces. However, pursuant to LAMC Section 12.21 A.4(c), non-residential projects within a Transit Priority Area (TPA), for which the project qualifies, may replace up to 30 percent of the required automobile parking spaces, or a reduction of 179 vehicle parking spaces, with bicycle parking at a rate of four bicycle parking spaces per vehicle parking space, thereby, further reducing the required vehicle parking spaces by 179 spaces. As such, the Project would provide a total of 716 bicycle parking spaces and 418 vehicle parking spaces. All short-term bicycle parking would be provided at the ground level, and all long-term bicycle parking would be valeted and provided at the roof level.

Vehicle Parking					
Use	SF	Required Ratio	Required Stalls	Commercial Reduction (20%)	TPA Reduction (30%)
Medical Office	140,305	5 per 1,000 SF	702	561	393
Retail	1,000	4 per 1,000 SF	4	3	2
Restaurant	4,000	10 per 1,000 SF	40	32	22
Total Minimum Required Spaces					417

Driveways and Loading Area

The Project proposes two, one-way, approximately 24-foot wide driveways accessed from the western frontage of the Project Site along South San Vicente Boulevard and one, two-way approximately 24-foot wide driveway accessed from the northern frontage of the Project Site along Orange Street. The South San Vicente Boulevard driveways serve as the visitor parking entrance, providing full valet services for all guests. The Orange Street driveway serves the employees of the Project, with valet services provided on Level 2 as to reduce queueing for employees. In addition, the Project includes a loading dock located on the northern frontage of the Project Site along Orange Street allowing loading vehicles to enter/exit the Project Site via South San Vicente Boulevard to the west or Sweetzer Avenue to the east. The loading area measures approximately 50 feet in depth and 25 feet in length, not including the trash compaction areas.

Sustainability Features

In compliance with the Los Angeles Green Building Code Section 99.05.211, the Project will provide a minimum of 30 percent of the total Code-required parking spaces capable of supporting future electric vehicle supply equipment (EVSE); where 10 percent of the total code-

required parking spaces will be installed with EV chargers to immediately accommodate electric vehicles within the parking areas. In addition, the Applicant is amenable to providing solar power in the form of either photovoltaic panels or thermal panels, where feasible and approved by the Department of Building and Safety.

Project Analysis (Site Planning and Design)

Citywide Design Guidelines

The Citywide Design Guidelines, adopted by the City Planning Commission on June 9, 2011, and last updated and adopted on October 24, 2019, establish a baseline for urban design expectations and present overarching design themes and best practices for residential, commercial, and industrial projects. Commission policy states that approved projects should either substantially comply with the Guidelines or through alternative methods to achieve the same objectives, and that the Guidelines may be used as a basis to condition an approved project. The design guidelines focus on three main design approaches: Pedestrian-First Design, 360 Degree Design, and Climate-Adaptive Design. These design guidelines focus on several areas of opportunity for attaining high quality design in mixed-use projects, including enhancing the quality of the pedestrian experience along the border of the project and public space; nurturing an overall active street presence; establishing appropriate height and massing within the context of the neighborhood; maintaining visual and spatial relationships with adjacent buildings; and optimizing high quality infill development that strengthens the visual and functional quality of the commercial environment.

Pedestrian-First Design

The Project would achieve Pedestrian-First Design goals by creating an active pedestrian experience along the Project's primary frontage along South San Vicente Boulevard as well as at the intersection of South San Vicente Boulevard, South Sweetzer Avenue, and Wilshire Boulevard. The entrances for the proposed medical office lobby, retail store, and restaurant spaces are located along the South San Vicente Boulevard frontage, as well as the driveways that provide guest vehicular and bicycle valet access. Due to lobby entrance and driveway configuration, the only entrances that are along the façade are those from the retail store and restaurant, the latter of which includes an outdoor dining plaza. The pedestrian experience is enhanced through upgrades to sidewalks, short-term bicycle parking, building lighting around the Project Site, and retention of and/or planting of street trees and landscaping. The building Design is also responsive to the pedestrian experience by locating the back of house uses, such as loading docks and employee vehicular and bike valet driveways, away from the front façade along South San Vicente Boulevard, and instead locating these uses along Orange Street, across the street from existing commercial uses.

360 Degree Design

In order to facilitate a 360 Degree Design, overall, the Project utilizes contemporary architectural styles and building materials, typical of recent high-rise office development along Wilshire and South San Vicente Boulevards. The four-level above-ground parking podium is fully screened and incorporates consistent building materials and architectural features of the medical office levels. In a departure from the standard building form, the Project proposes open space balconies and a reduction in floor plate size, as shown in the Project Plans, concentrating the increase in vertical height towards the intersection of South San Vicente Boulevard, South Sweetzer Avenue, and Wilshire Boulevard and away from the adjacent commercial and residential uses to the north and east of the Project Site.

Climate-Adaptive Design

The Project would achieve Climate-Adaptive Design by complying with current regulations regarding building design, solar installation, water-wise landscape, and electric vehicle (EV) parking requirements. Likewise, although not required, the Project proposes both hardscaped and landscape areas to provide open space access to guests and employees of the buildings uses.

Overall, the design, scale, massing, and style of the buildings is appropriate in the context of the neighborhood, as the Wilshire Community area is currently developed and undergoing residential, commercial, institutional and mixed-use development with a variety of mid-, and high-rise buildings which are adjacent to public transit, including the future Metro D (Purple) Line and rapid and local stations in addition to regional and neighborhood-serving institutional, residential, commercial, and entertainment uses.

Urban Design Studio: Professional Volunteer Meeting

The Project was reviewed by the Department of City Planning's Urban Design Studio - Professional Volunteer Program (PVP) on October 6, 2020. The following issues, concerns, and recommendations were discussed:

Pedestrian-Friendly Design:

- Expand the sidewalk to improve the pedestrian experience along the South San Vicente Boulevard frontage road.
- Make the alleyway more accessible and comfortable for pedestrians.
- Incorporate more public space into the ground floor by setting back the building, opening up the commercial space, and/or adding more outdoor seating.

360-Degree Design:

- Reconsider number of above-grade parking as each parking level is 1.5 times taller than the average parking level height.
- Integrate more open space into the roof areas.
- Consider greater variation in building articulation and massing, particularly at on the corner of Wilshire and San Vicente Boulevards.

Climate-Adapted Design:

- Provide information regarding solar energy use and other sustainability features on the Project Plans.
- Consider the buildings impacts on birds, urban heat island, and glare, if not otherwise discussed in the EIR.

Project Design Changes

Based on feedback from the Professional Volunteer Program (PVP) and the Urban Design Studio, the Applicant did not make any refinements to the project plans. However, many of the recommendations are in regard to changes that would significantly alter the function and feasibility of the Project, namely additional ground-floor setbacks or plazas, changes to massing, rooftop open space, and the height of parking floors which have been designed to be at a height in conformance with the City Planning Commission's Above-Grade Parking Advisory. Recommended changes for additional setbacks or plazas or changes in massing may also

result in an increased building height. Therefore, given the type and scale of PVP's recommendations, no major design changes were considered by the Applicant. Site Plan Review Conditions of Approval have been proposed to address sustainability features such as EV-parking, construction generators, stormwater/irrigation, and building materials related to light and glare.

Entitlement Analysis

General Plan Amendment, Vesting Zone and Height District Change

The approximately .74-acre Project Site is located within the Wilshire Community Plan area and is comprised of seven lots. The Community Plan designates the entire Project Site as Limited Commercial. According to the Community Plan, corresponding zones for the Limited Commercial designation are C1, CR, C1.5, P, RAS3, RAS4. The existing C1 zoning is consistent with this designation. In addition, the C1 zone combined with Height District 1VL allows for a maximum building height of 45 feet and three stories and a maximum FAR of 1.5:1. The Project is requesting a General Plan Amendment to Regional Commercial, with corresponding zones of CR, C1.5, C2, C4, P, PB, RAS3, RAS4, R3, R4, and R5, and a Vesting Zone and Height District Change from C1-1VL-O to (T)(Q)C2-2D-O which would permit a maximum FAR of 4.5:1, and would establish a maximum building height of 230 feet through a D limitation to Height District 2, which otherwise does not limit building height. Additionally, the Project's request includes, pursuant to LAMC Section 12.32 P, a reduction in parking not to exceed 20 percent for a commercial project.

The General Plan Amendment, Zone and Height District change would allow for a new mixed-use commercial development, comprised of a single medical office tower with ground floor commercial spaces, consistent with commercial development along the Wilshire Boulevard corridor, within an area identified as a Regional Center by the General Plan Framework, and in close proximity to the Metro D (Purple Line) station.

In addition, in accordance with California Government Code Section 66300 (b)(1), plan amendments and zone changes for properties which allow for residential development generally cannot reduce the residential capacity of a site to below that which existed on January 1, 2018 (the existing C1-1VL-O zone). Therefore, Q-condition No. 2 is proposed to ensure that the existing by-right residential capacity of the site can still be retained. If future residential development is proposed on the site, it would nonetheless be subject to any applicable entitlement review and associated CEQA review (such as the Site Plan Review process for projects with over 50 residential units).

Site Plan Review

In addition, the Project is subject to Site Plan Review approval as it is a development project which results in an increase in over 50,000 square feet of commercial floor area. Proposed Site Plan Review Conditions of Approval include requirements for conformance with the submitted plans, landscaping and trees, sustainability, valet operations, and standards for the design of above-ground parking areas for potential conversion to other uses, consistent with the City Planning Commission's Above Ground Parking Advisory.

Environmental Impact Report

The City of Los Angeles released the Final EIR (SCH No. 2020010172) dated January 2022, and the Erratum dated February 2022 detailing the relevant environmental impacts resulting from the Project. The Certified EIR also includes the Draft EIR for the 656 South San Vicente Medical Office Project published on June 17, 2021.

The EIR identified the following significant and unavoidable impacts: Noise (Project and Cumulative On-Site Construction Noise, Cumulative Off-Site Construction Noise, and Project On-Site Construction Vibration Human Annoyance).

The EIR was certified by the Deputy Advisory Agency (DAA) on May 3, 2022, in conjunction with the approval of Case No. VTT-74865. The decision of the DAA was subsequently appealed and is pending decision by the City Planning Commission immediately prior to the subject case.

ISSUES

Public Hearing and Testimony

On February 2, 2022 and March 16 2022 at 9:30 AM, public hearings were conducted by the Hearing Officer on this matter, on behalf of the City Planning Commission, telephonically via Zoom. There was a total of 55 participants, at maximum and with some overlapping, during the meeting including City Planning Staff, the Applicant team, and members of the public. Forty members of the public spoke at the hearing; five people spoke in support of the project and thirty-five (35) people spoke in opposition to the Project. In addition, a total of five written comments were received ahead of the hearings. Support for the project generally focused on the City's critical need for additional medical office space and job density within the area of the future Metro D (Purple) Line station. Opposition to the project centered on the scale of the Project, the individual and public health impacts related to construction and operation, a perceived increase in traffic and congestion related to intensity of uses on the Project Site, the need for additional parking on-site, alley access and associated potential vehicular conflicts, lack of local hire provisions, and the potential of the Project to discourage renting in adjacent properties due to construction activities.

A further detailed summary of the comments and correspondence received is in the Public Hearing and Communications Section, Page P-1. The issues raised are addressed below.

Scale, Height, and Adjacent Low-Rise Residential Uses

As previously mentioned, the Project Site is adjacent to existing, two-story, multi-family residential buildings constructed in the late 1920's to 1940's across a shared alleyway to the east, while taller buildings are located along the adjacent commercial corridors. The scale and height of the Project have been concerns to the neighboring community members, despite similar-sized buildings having been constructed over the past 50 years along Wilshire Boulevard. Likewise, more recently, there have been similar-sized developments further northwest along South San Vicente Boulevard towards the intersection of South San Vicente and 3rd Street, adjacent to the Cedars Sinai Medical Complex. The Project would be consistent with the scale and height of other office development along the commercial corridors.

CEQA Analysis

Environmental impacts related to Project construction, operation, and traffic generation were frequently raised by members of the public as potential detrimental causes of worsening public

health in the adjacent buildings and surrounding neighborhood throughout the Project timeline. Potential environmental impacts were analyzed in the EIR prepared for the Project. Additionally, comments submitted by the public regarding the contents of the EIR were responded to at length and in their entirety in the Final EIR prepared for the Project. While Staff recognizes the concerns of the public, the environmental analysis, whose findings are included as part of this report, adequately address any potential impacts related to Project construction and operation. Likewise, Mitigation Measures have been included to reduce impacts to the less than significant, where feasible. Any significant and unavoidable impacts related to the Project have been disclosed and did not include any impacts associated with air quality, transportation, public services, or any other impacts directly related to public health, with the exception of construction noise, which is temporary in nature.

Traffic and Parking Intrusion into Residential Neighborhoods

Potential cut-through traffic and the reduced amount of parking provided by the Project has been a concern to the neighboring community due to a perceived impact regarding traffic congestion and to parking on adjacent residential surface streets.

The Project's non-CEQA transportation analysis included a Residential Street Segment Analysis (Appendix J of the Draft EIR, page 80-81), in accordance with Department of Transportation's Transportation Assessment Guide (TAG), to determine cut-through traffic impacts and volumes on nearby residential streets. The Project-related increase along the segment of Orange Street between Sweetzer Avenue and La Jolla Avenue would result in the street being deemed excessively burdened based on the TAG standards. It is important to note that Orange Street provides direct access to the Project Site and the projected final volumes along Orange Street show that the street would still operate and function as a Local Street. The traffic study for the project recommended the implementation of a Neighborhood Transportation Management Plan (NTMP). However, since traffic impacts related to queuing and Level of Service (LOS) are non-CEQA impacts, mitigation under CEQA is not required. Instead, DOT's non-CEQA measures, such as the NTMP, are typically included under the authority of the Site Plan Review entitlement. As such, the recommended NTMP is included as a Site Plan Review Condition No. 16.

The NTMP would be developed in cooperation with LADOT, Council Office staff, and affected neighborhood residents. The NTMP would include an implementation plan that sets key milestones and identifies a proposed process in developing a NTMP for the location. Typical NTM physical measures may include, but are not limited to, traffic circles, speed humps, installation of barriers, speed tables, chicanes, chokers, roadway narrowing effects (raised medians, etc.), landscaping features, roadway striping changes, and or operational measures such as turn restrictions, speed limits, and installation of stop signs. Additionally, the Applicant has expressed a willingness to assist the neighboring community to convert all applicable street parking to permit parking.

The Project would also implement a Transportation Demand Management (TDM) as part of its Mitigation Monitoring Program to encourage the use of alternate transportation to help reduce traffic amounts in general. The Transportation Analysis also demonstrated that the Project would be consistent with relevant transportation goals and policies and would not result in other traffic impacts or vehicular conflicts.

There have also been requests for the Project to provide more parking, as well as Appeals to the Vesting Tentative Tract Map that have referred to the reduction in parking as being misrepresented, which have been responded to at length in a separate Appeal Report provided in tandem with this Staff Report. As discussed above, pursuant to LAMC Section 12.32 P, the Project is requesting a reduction in parking not to exceed 20 percent, incident to a legislative

action, reducing the required vehicle parking to a total 597 spaces. Pursuant to LAMC Section 12.21 A.16(a)(2) the Project is required to provide short-term and long-term parking spaces for all commercial uses, for a total of 58 bicycle parking spaces. However, pursuant to LAMC Section 12.21 A.4(c), non-residential projects within a TPA may replace up to 30 percent of the required automobile parking spaces, or a reduction of 179 vehicle parking spaces, with bicycle parking at a rate of four bicycle parking spaces per vehicle parking space, thereby, further reducing the required vehicle parking spaces by 179 spaces. As such, the Project would provide a total of 716 bicycle parking spaces and 418 vehicle parking spaces.

In addition, response to community comments on the matter, the Applicant has prepared two Supplemental Parking Analysis documents. The first, dated January 4, 2022, discusses the potential of the Project to provide up to 33 additional parking spaces through unstriped aisle, tandem, and other parking spaces, to be able to accommodate the anticipated peak parking demand during both the weekday and weekend. The second, dated January 31, goes further to discuss potential refinements to the projects known as the “Refined Project” to determine if changes to uses within the building could potentially reduce parking demand. The “Refined Project” analyzed included 140,305 square feet of medical office, consisting of 28,061 square feet of medical laboratory space (approximately 20% of the total) and 112,244 square feet of medical office space, and approximately 5,000 square feet of commercial restaurant and retail/pharmacy uses. Consistent with the Project, the “Refined Project” would provide a total of 418 vehicular parking spaces, plus up to 33 additional parking spaces through unstriped aisle, tandem, and other parking spaces, for a total of 451 spaces, with full valet operations. No changes to the vehicular or pedestrian access were proposed under the Refined Project. By incorporating medical laboratory space rather than medical office space, this would result in less peak parking demand as compared to the proposed Project. The parking analysis demonstrated that under the “Refined Project”, parking supply would be able to accommodate the anticipated parking demands throughout the day for both weekday and weekend conditions and therefore potential overflow parking on adjacent street parking could be reduced by including medical laboratory uses in lieu of medical office uses.

Nonetheless, the Applicant is currently proceeding with the Project for a primarily medical office use building, without restrictions requiring a minimum for medical laboratory uses. While the Project proposes a significant reduction in parking, its location in proximity to public transit, proposed number of employees, and TDM strategies, are both justified and accurately represented. In addition, pursuant to PRC Section 21099 (d)(1), that states a project’s “aesthetic and parking impacts of a residential, mixed-use residential, or employment center project on an infill site within a transit priority area shall not be considered significant impacts on the environment.” As such, parking impacts would not be considered significant under CEQA. The applicant may also elect to include medical laboratory uses in lieu of some of the medical office space, in consideration of the study’s findings and market demands.

Local Hire

While concerns were voiced regarding the absence of local hire provisions, local hire provisions are not required by any applicable federal, state, or local law, nor as a requirement related to the requested entitlements. Nonetheless, the Project Applicant has stated that they have signed a Letter of Intent for a provision of local hire.

Rent Discouragement

While Staff recognizes the concerns of the public related to the perceived loss of renters due to construction activities, potential economic-related impacts to adjacent uses is neither required to be reviewed by State or Federal law, nor is there currently a codified standard set by the City

with which to analyze the potential for a project to discourage renter or owner occupancy in adjacent uses. In addition, construction activities regularly occur throughout the City of Los Angeles. As such, there is not substantial evidence to support the assertion that the Project, during construction, would increase rental vacancy in the surrounding neighborhood in both the long- and short-term.

CONCLUSION

Through the development of a new mixed-use, medical office and retail/restaurant development the Project would provide needed medical office space and jobs within close proximity to the future Metro D (Purple Line) Wilshire/La Cienega Station and Cedars Sinai Medical Center. The Project would create a more walkable and vibrant neighborhood by supplying additional medical facilities, retail space, and restaurant uses, improving adjacent sidewalks and public rights-of-way, consistent with goals and objectives outlined in the General Plan Framework and Wilshire Community Plan.

In consideration of all the facts and mandatory findings for the requested entitlements, City Planning Staff recommends that the City Planning Commission approve the Project and the following entitlement requests: the General Plan Amendment to re-designate the underlying land use from Limited Commercial to Regional Commercial; a Vesting Zone Change and Height District Change, from C1-1VL-O to (T)(Q)C2-2D-O to allow for an FAR of 4.5:1 and up to a 20 percent reduction in vehicle parking; and a Site Plan Review for the development of a project resulting in an increase greater than 50,000 square feet of non-residential floor area.

CONDITIONS FOR EFFECTUATING (T) TENTATIVE CLASSIFICATION REMOVAL

Pursuant to Section 12.32 G of the Municipal Code, the (T) Tentative Classification shall be removed by the recordation of a final parcel or tract map or by posting of guarantees through the B-permit process of the City Engineer to secure the following without expense to the City of Los Angeles, with copies of any approval or guarantees provided to the Department of City Planning for attachment to the subject planning case file.

Dedications/Improvements and Responsibilities/Guarantees.

Dedications and Improvements herein contained in these conditions which are in excess of street improvements contained in either the Mobility Element 2035 or any future Community Plan amendment or revision may be reduced to meet those plans with the concurrence of the Department of Transportation and the Bureau of Engineering:

1. As part of early consultation, plan review, and/or project permit review, the applicant/developer shall contact the responsible agencies to ensure that any necessary dedications and improvements are specifically acknowledged by the applicant/developer.
2. **Bureau of Engineering.** Street Dedications and Improvements shall be provided to the satisfaction of the City Engineer.
3. **Sewer.** Construction of necessary sewer facilities, or payment of sewer fees, shall be to the satisfaction of the City Engineer.
4. **Drainage.** Construction of necessary drainage and storm water runoff drainage facilities to the satisfaction of the City Engineer.
5. **Driveway/Parkway Area Plan.** Preparation of a parking plan and driveway plan to the satisfaction of the appropriate District Offices of the Bureau of Engineering and the Department of Transportation.
6. **Fire.** Incorporate into the building plans the recommendations of the Fire Department relative to fire safety, which includes the submittal of a plot plan for approval by the Fire Department either prior to the recordation of a final map or the approval of a building permit.
7. **Cable.** Make any necessary arrangements with the appropriate cable television franchise holder to assure that cable television facilities will be installed in City rights-of-way in the same manner as is required of other facilities, pursuant to Municipal Code Section 17.05 N to the satisfaction of the Department of Telecommunications.
8. **Lighting.** Street lighting facilities shall be provided to the satisfaction of the Bureau of Street Lighting.
9. **Street Trees.** All trees in the public right-of-way shall be provided per the current Urban Forestry Division Standards.

Notice: Certificates of Occupancy for the subject property will not be issued by the City until the construction of all the public improvements (streets, sewers, storm drains, etc.), as required herein, are completed to the satisfaction of the City Engineer.

(Q) QUALIFIED CONDITIONS

Pursuant to Section 12.32 G of the Municipal Code, the following limitations are hereby imposed upon the use of the subject property, subject to the “Q” Qualified classification.

1. **Site Development.** The use and development of the property shall be in substantial conformance with the plans submitted with the application and marked Exhibit A, dated June 23, 2022. No change to the plans will be made without prior review by the Department of City Planning, and written approval by the Director of Planning. Each change shall be identified and justified in writing. Minor deviations may be allowed in order to comply with the provisions of the Municipal Code or the project conditions. The Project shall be constructed in a manner consistent with the following:

- a) A maximum of 140,305 square feet of medical office and/or lab floor area; and
- b) A maximum of 5,000 square feet of commercial floor area.

Additional floor area may be permitted if: above-ground parking areas are repurposed to commercial uses and the site's Floor Area Ratio (FAR) does not exceed 6:1, subject to the Q clarification procedures of LAMC Section 12.32-H or other appropriate LAMC required process, and subject to any applicable CEQA review.

2. **Residential Capacity.** Notwithstanding the above, in accordance with California Government Section 66300 (b)(1), the site may also be developed with residential uses allowed and in accordance with the density and all other development standards of the C1-1VL-O zone, as in effect on January 1, 2018.
3. **Parking.** A 20 percent reduction from the parking requirements of the LAMC is permitted.

D LIMITATIONS

Pursuant to Section 12.32 G of the Municipal Code, the following limitations are hereby imposed upon the use of the subject property, subject to the “D” Development Limitations.

A. Development Limitations:

1. **Floor Area Ratio.** The total floor area over the Project Site shall not exceed a 4.5:1 floor area ratio (FAR), or a total of 145,305 square feet. The FAR may be increased if: above-ground parking areas are repurposed to commercial uses and the site’s FAR does not exceed 6:1, subject to any applicable CEQA review and implementation.
2. **Building Height.** Building height shall be limited to a maximum of 230 feet, consistent with Exhibit “A”, dated June 23, 2022.

CONDITIONS OF APPROVAL

Site Plan Review Conditions

1. **Site Development.** The use and development of the property shall be in substantial conformance with the plans submitted with the application and marked Exhibit A, dated June 23, 2022. No change to the plans will be made without prior review by the Department of City Planning, Major Projects Section, and written approval by the Director of Planning. Each change shall be identified and justified in writing. Minor deviations may be allowed in order to comply with the provisions of the Municipal Code or the project conditions. The project shall be in substantial conformance with the following description:

The development of 140,305 square feet of medical office space, 4,000 square feet of restaurant/retail space, and 1,000 square feet for other commercial uses, such as a pharmacy. The proposed uses would be built within a single, 12-story building that includes ground floor lobby and commercial space.

2. **Development Services Center.** Prior to sign-off on building permits by the Department of City Planning's Development Services Center for the project, the Department of City Planning's Major Projects Section shall confirm, via signature on the plans, that the project's building plans substantially conform to the conceptual plans stamped as Exhibit A, dated June 23, 2022, as approved by the City Planning Commission.

Note to Development Services Center: The plans presented to, and approved by, the City Planning Commission (CPC) included specific architectural details that were significant to the approval of the project. Plans submitted at plan check for condition clearance shall include a signature and date from Major Projects Section planning staff to ensure plans are consistent with those presented at CPC.

3. **Parking.** Prior to the issuance of a building permit, the driveway and parking plan shall be submitted for review and approval to the Department of Transportation, and shall be in substantial compliance with the following:
 - a. **Valet.** All vehicle and off-street bicycle parking shall be provided with full valet services at no cost to the guests/visitors and employees.
 - b. **Design.**
 - i. Any above grade parking shall be designed to be utilized and easily repurposed to other uses.
 - ii. The height of each parking level shall have sufficient clearance to be adaptable to non-parking uses. Once converted, the building shall permit a minimum floor to ceiling height of nine feet for commercial uses.
 - iii. All above grade levels of the Project's parking podium shall be wrapped or incorporated into the architecture or landscaping.

4. **Electric Vehicle Parking.** All electric vehicle charging spaces (EV Spaces) and electric vehicle charging stations (EVCS) shall comply with the regulations outlined in Chapter IX, Article 9, LAMC Sections 99.04.106 and 99.05.106.
5. **Landscaping.** Prior to the issuance of a building permit, a landscape and irrigation plan prepared in accordance with LAMC Sections 12.40 through 12.43 and shall be submitted to the Department of City Planning for approval. The landscape plan shall be in substantial conformance with the landscape plan stamped Exhibit A, dated June 23, 2022. Minor deviations from the requirements provided below may be permitted by the Department of City Planning to permit the existing landscaping conditions provided that the plantings are well established and in good condition.
6. **Trees.** The Applicant shall provide a minimum of seven trees on-site and/or in the parkway, to the satisfaction of Urban Forestry.
 - a. **Tree Wells.**
 1. The minimum depth of tree wells shall be as follows:
 - a. Minimum depth for trees shall be 42 inches.
 - b. Minimum depth for shrubs shall be 30 inches.
 - c. Minimum depth for herbaceous plantings and ground cover shall be 18 inches.
 - d. Minimum depth for an extensive green roof shall be three inches.
 2. The minimum amount of soil volume for tree wells shall be based on the size of the tree at maturity as follows:
 - a. 220 cubic feet for a tree 15-19 feet tall at maturity.
 - b. 400 cubic feet for a tree 20-24 feet tall at maturity.
 - c. 620 cubic feet for a tree 25-29 feet tall at maturity.
 - d. 900 cubic feet for a tree 30-34 feet tall at maturity.
 - b. Any trees that are required pursuant to LAMC Section 12.21 G and are planted on any podium or deck shall be planted in a minimum three-foot planter.
7. **Tree Maintenance.** New trees planted within the public right-of-way shall be spaced not more than an average of 30 feet on center, unless otherwise permitted by the Urban Forestry Division, Bureau of Public Works.
8. **Construction Signage.** There shall be no off-site commercial signage on construction fencing during construction.
9. **Glare.** The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat.
10. **Reflectivity.** Glass used in building façades shall be non-reflective or treated with a non-reflective coating in order to minimize glare from reflected sunlight.

11. **Construction Generators.** The Project contractor shall use power construction equipment with state-of-the-art noise shielding and muffling devices. The Project's construction contractor shall use on-site electrical sources and solar generators to power equipment rather than diesel generators, where feasible.
12. **Stormwater/Irrigation.** The project shall implement on-site stormwater infiltration as feasible based on the site soils conditions, the geotechnical recommendations, and the City of Los Angeles Department of Building and Safety Guidelines for Storm Water Infiltration. If on-site infiltration is deemed infeasible, the project shall analyze the potential for stormwater capture and reuse for irrigation purposes based on the City Low Impact Development (LID) guidelines.
13. **Mechanical Equipment.** All mechanical equipment shall be fully screened from view of any abutting properties and the public right-of-way.
14. **Trash/Storage.** All trash collecting and storage areas shall be located on-site and not visible from the public right-of-way. Trash receptacles shall be enclosed and/or covered at all times. Trash/recycling containers shall be locked when not in use.
15. **Utilities.** All utilities shall be fully screened from view of any abutting properties and the public right-of-way.
16. **Neighborhood Traffic Management Program (NTMP).** In order to address potential impacts for residential street segments (Orange Street between Sweetzer Avenue and La Jolla Avenue) and to address off-street parking, the project would fund implementation of an NTMP, up to an amount not to exceed \$100,000. The NTMP would be developed in cooperation with LADOT, Council District 5, and affected neighborhood residents. The NTMP would include an implementation plan that sets key milestones and identifies a proposed process in developing a NTM plan for the location. Typical NTM physical measures may include, but are not limited to, traffic circles, speed humps, installation of barriers, speed tables, chicanes, chokers, roadway narrowing effects (raised medians, etc.), landscaping features, roadway striping changes, and or operational measures such as turn restrictions, speed limits, and installation of stop signs.

The NTMP would be formalized through an agreement between the Applicant and LADOT prior to the issuance of the Certificate of Occupancy. The agreement should include a funding guarantee and outreach process, selection and approval criteria for any evaluated NTM measures and an implementation phasing plan.

The final implementation plan, if consensus is reached among the stakeholders, would be subject to review and approval by LADOT's District Office and it would be the Applicant's responsibility to implement any approved NTM measures through the BOE B-permit process.

Environmental Conditions

17. **Implementation.** The Mitigation Monitoring Program (MMP), attached as "Exhibit B" and part of the case file, shall be enforced throughout all phases of the Project. The Applicant shall be responsible for implementing each Project Design Features (PDF) and Mitigation Measure (MM) and shall be obligated to provide certification, as identified below, to the appropriate monitoring and enforcement agencies that each PDF and MM has been implemented. The Applicant shall maintain records demonstrating compliance with each PDF and MM. Such records shall be made available to the City upon request.

18. **Construction Monitor.** During the construction phase and prior to the issuance of building permits, the Applicant shall retain an independent Construction Monitor (either via the City or through a third-party consultant), approved by the Department of City Planning, who shall be responsible for monitoring implementation of PDFs and MMs during construction activities consistent with the monitoring phase and frequency set forth in this MMP.

The Construction Monitor shall also prepare documentation of the Applicant's compliance with the PDFs and MMs during construction every 90 days in a form satisfactory to the Department of City Planning. The documentation must be signed by the Applicant and Construction Monitor and be included as part of the Applicant's Compliance Report. The Construction Monitor shall be obligated to immediately report to the Enforcement Agency any non-compliance with the MMs and PDFs within two businesses days if the Applicant does not correct the non-compliance within a reasonable time of notification to the Applicant by the monitor or if the non-compliance is repeated. Such non-compliance shall be appropriately addressed by the Enforcement Agency.

19. **Substantial Conformance and Modification.** After review and approval of the final MMP by the Lead Agency, minor changes and modifications to the MMP are permitted, but can only be made subject to City approval. The Lead Agency, in conjunction with any appropriate agencies or departments, will determine the adequacy of any proposed change or modification. This flexibility is necessary in light of the nature of the MMP and the need to protect the environment. No changes will be permitted unless the MMP continues to satisfy the requirements of CEQA, as determined by the Lead Agency.

The Project shall be in substantial conformance with the PDFs and MMs contained in this MMP. The enforcing departments or agencies may determine substantial conformance with PDFs and MMs in the MMP in their reasonable discretion. If the department or agency cannot find substantial conformance, a PDF or MM may be modified or deleted as follows: the enforcing department or agency, or the decision maker for a subsequent discretionary project related approval finds that the modification or deletion complies with CEQA, including CEQA Guidelines Sections 15162 and 15164, which could include the preparation of an addendum or subsequent environmental clearance, if necessary, to analyze the impacts from the modifications to or deletion of the PDFs or MMs. Any addendum or subsequent CEQA clearance shall explain why the PDF or MM is no longer needed, not feasible, or the other basis for modifying or deleting the PDF or MM, and that the modification will not result in a new significant impact consistent with the requirements of CEQA. Under this process, the modification or deletion of a PDF or MM shall not, in and of itself, require a modification to any Project discretionary approval unless the Director of Planning also finds that the change to the PDF or MM results in a substantial change to the Project or the non-environmental conditions of approval.

20. **Tribal Cultural Resource Inadvertent Discovery.** In the event that objects or artifacts that may be tribal cultural resources are encountered during the course of any ground disturbance activities (excavating, digging, trenching, plowing, drilling, tunneling, quarrying, grading, leveling, removing peat, clearing, driving posts, auguring, backfilling, blasting, stripping topsoil or a similar activity), all such activities shall temporarily cease on the Project Site until the potential tribal cultural resources are properly assessed and addressed pursuant to the process set forth below:
 - a. Upon a discovery of a potential tribal cultural resource, the Applicant shall immediately stop all ground disturbance activities and contact the following: (1) all California Native American tribes that have informed the City they are

traditionally and culturally affiliated with the geographic area of the proposed project; (2) and the Department of City Planning at (213) 473-9723.

- b. If the City determines, pursuant to Public Resources Code Section 21074 (a)(2), that the object or artifact appears to be tribal cultural resource, the City shall provide any effected tribe a reasonable period of time, not less than 14 days, to conduct a site visit and make recommendations to the Applicant and the City regarding the monitoring of future ground disturbance activities, as well as the treatment and disposition of any discovered tribal cultural resources.
- c. The Applicant shall implement the tribe's recommendations if a qualified archaeologist and by a culturally affiliated tribal monitor, both retained by the City and paid for by the Applicant, reasonably concludes that the tribe's recommendations are reasonable and feasible.
- d. The Applicant shall submit a tribal cultural resource monitoring plan to the City that includes all recommendations from the City and any effected tribes that have been reviewed and determined by the qualified archaeologist and by a culturally affiliated tribal monitor to be reasonable and feasible. The Applicant shall not be allowed to recommence ground disturbance activities until this plan is approved by the City.
- e. If the Applicant does not accept a particular recommendation determined to be reasonable and feasible by the qualified archaeologist or by a culturally affiliated tribal monitor, the Applicant may request mediation by a mediator agreed to by the Applicant and the City who has the requisite professional qualifications and experience to mediate such a dispute. The Applicant shall pay any costs associated with the mediation.
- f. The Applicant may recommence ground disturbance activities outside of a specified radius of the discovery site, so long as this radius has been reviewed by the qualified archaeologist and by a culturally affiliated tribal monitor and determined to be reasonable and appropriate.
- g. Copies of any subsequent prehistoric archaeological study, tribal cultural resources study or report, detailing the nature of any significant tribal cultural resources, remedial actions taken, and disposition of any significant tribal cultural resources shall be submitted to the South Central Coastal Information Center (SCCIC) at California State University, Fullerton.

Administrative Conditions

1. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review or approval, plans, etc., as may be required by the subject conditions, shall be provided to the Planning Department for placement in the subject file.
2. **Code Compliance.** Area, height and use regulations of the zone classification of the subject property shall be complied with, except where herein conditions are more restrictive.
3. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on

any subsequent property owners, heirs or assign. The agreement must be submitted to the Planning Department for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Planning Department for attachment to the file.

4. **Definition.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public officials, legislation or their successors, designees or amendment to any legislation.
5. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Planning Department and any designated agency, or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.
6. **Building Plans.** Page 1 of the grants, and any subsequent pages that include grants, and all of the conditions of approval shall be printed on the building plans submitted to the Department of City Planning and the Department of Building and Safety.
7. **Project Plan Modifications.** Any corrections and/or modifications to the project plans made subsequent to this grant that are deemed necessary by the Department of Building and Safety, Housing Department, or other Agency for Code compliance, and which involve a change in Site Plan, floor area, parking, building height, yards or setbacks, building separations, or lot coverage, shall require a referral of the revised plans back to the Department of City Planning for additional review and final sign-off prior to the issuance of any building permit in connection with said plans. This process may require additional review and/or action by the appropriate decision-making authority including the Director of Planning, City Planning Commission, Area Planning Commission, or Board.
8. **Indemnification and Reimbursement of Litigation Costs.** The Applicant shall do all of the following:
 - i. Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
 - ii. Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
 - iii. Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).

- iv. Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- v. If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions includes actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

FINDINGS

ENTITLEMENT FINDINGS

General Plan/Charter Findings

1. The General Plan Amendment Will Further the Purposes, Intent and Provisions of the General Plan (Charter Section 556).

The Director-initiated General Plan Amendment and requested Zone and Height District Change are in substantial conformance with the purposes, intent, and provisions of the General Plan as explained below:

General Plan Land Use Designation

The Project Site is located within the Wilshire Community Plan, which was adopted by the City Council on September 19, 2001. The 0.74-acre, trapezoid-shaped Project Site is generally bounded by Orange Street to the north, a shared alleyway to the northeast, Sweetzer Avenue to the east, and South San Vicente Boulevard to the southwest. The Community Plan currently designates the Project Site for Limited Commercial land uses, corresponding to the C1 Zone. The site is zoned C1-1VL-O (Commercial Zone, Height District 1 Very Limited, Oil Drilling District). The C1 Zone allows for retail stores up to 100,000 square feet of floor area offices, businesses, hotels, hospitals/clinics, museums, and multi-family residential uses. No setbacks are required for front yards, side yards, and rear yards for commercial uses in the C1 Zone. Height District 1VL imposes a 45-foot height limit for commercial uses and permits up to three stories, and allows for a Floor Area Ratio of 1.5:1.

As proposed, the General Plan Amendment would re-designate the Project Site from Limited Commercial to Regional Commercial land uses. In addition, a Vesting Zone Change and Height District Change would modify the existing zoning from C1-1VL-O to (T)(Q)C2-2D-O. The proposed Regional Commercial land use designation has the following corresponding zones: CR, C1.5, C2, C4, P, PB, RAS3, RAS4, R3, R4, and R5. Thus, the recommended (T)(Q)C2-2D-O Zone would be consistent with the adoption of the proposed land use designation and in substantial conformance with the purpose, intent, and provisions of the General Plan as it is reflected within the Wilshire Community Plan.

General Plan Text

The Los Angeles General Plan sets forth goals, objectives and programs that guide both citywide and community specific land use policies. The General Plan is comprised of a range of State-mandated elements, including, but not limited to, Land Use, Transportation, Noise, Safety, Housing and Conservation. The City's Land Use Element is divided into 35 community plans that establish parameters for land use decisions within those sub-areas of the City. The Project is in compliance with the following Elements of the General Plan: Framework Element, Mobility Element, Health and Wellness and Air Quality Elements and the Land Use Element – Wilshire Community Plan.

Framework Element

The Framework Element for the General Plan (Framework Element) was adopted by the City of Los Angeles in December 1996 and re-adopted in August 2001. The Framework Element provides guidance regarding policy issues for the entire City of Los Angeles, including the Project Site. The Framework Element also sets forth a Citywide comprehensive long-range growth strategy and defines Citywide policies regarding such issues as land use, housing, urban form, neighborhood design, open space, economic development, transportation, infrastructure, and public services. The General Plan Amendment, Vesting Zone Change, Height District Change, and Proposed Project are consistent with the goals, objectives, and policies of the General Plan Framework.

The City of Los Angeles General Plan Framework Long Range Land Use Diagram generally identifies the properties adjacent to the Project Site to the east along the Wilshire Boulevard corridor as a Regional Commercial. Regional Centers are considered a focal point of regional commerce, identity and activity and contain a diversity of uses such as corporate and professional offices, residential, retail commercial malls, government buildings, major health facilities, major entertainment and cultural facilities and supporting services. Generally, different types of Regional Commercial uses will fall within the range of floor area ratios from 1.5:1 to 6:1 and are characterized by developments six- to 20-stories (or higher) in height.

The Proposed Project involves the demolition of the two existing buildings and surface parking lots, and the construction of a mixed-use medical office building with up to 145,305 square feet of new floor area. The Project Site is currently improved with two buildings and associated surface parking lots, comprised of a 5,738 square-foot, vacant educational building, and an 8,225 square foot Big 5 Sporting Goods store, combined totaling approximately 13,963 square feet of existing floor area. The Project proposes 140,305 square feet of medical office space, 4,000 square feet of restaurant space, and 1,000 square feet for other commercial uses, such as a pharmacy. The proposed uses would be built within a single, 12-story building that includes ground floor lobby and commercial space, four levels of podium parking, and seven levels of medical office uses. The Project is consistent with and supports the Regional Center as it includes the development of a major health facility with ground floor commercial and restaurant uses. Additionally, consistent with the Framework Element's Regional Centers, the Project proposes a 12-story tower with a FAR of 4.5:1.

Chapter 3: Land Use

The Land Use Chapter of the Framework Element identifies objectives and supporting policies relevant to the Project Site. Those objectives and policies seek, in part, to encourage the development of commercial and residential uses and structures that integrate housing units with commercial uses. The Project supports and will be generally consistent with the General Plan Framework Land Use Chapter as it accommodates development of commercial uses in accordance with the applicable policies of the Wilshire Community Plan. Specifically, the Project would be consistent with the following goals, objective and policies, as set forth in the General Plan Framework Land Use Chapter:

Goal 3A: A physically balanced distribution of land uses that contributes towards and facilitates the City's long-term fiscal and economic viability, revitalization of economically depressed areas, conservation of existing residential neighborhoods, equitable distribution of public resources, conservation of natural resources, provision of adequate infrastructure and public services, reduction of traffic congestion and improvement of air

quality, enhancement of recreation and open space opportunities, assurance of environmental justice and a healthful living environment, and achievement of the vision for a more livable city.

Objective 3.1: *Accommodate a diversity of uses that support the needs of the City's existing and future residents, businesses, and visitors.*

Policy 3.1.1: *Identify areas on the Long-Range Land Use Diagram and in the community plans sufficient for the development of a diversity of uses that serve the needs of existing and future residents (housing, employment, retail, entertainment, cultural/institutional, educational, health, services, recreation, and similar uses), provide job opportunities, and support visitors and tourism.*

Objective 3.2: *Provide for the spatial distribution of development that promotes an improved quality of life by facilitating a reduction of vehicular trips, vehicle miles traveled, and air pollution.*

Policy 3.2.3: *Provide for the development of land use patterns that emphasize pedestrian/bicycle access and use in appropriate locations.*

Objective 3.3: *Accommodate projected population and employment growth within the City and each community plan area and plan for the provision of adequate supporting transportation and utility infrastructure and public services.*

Objective 3.4: *Encourage new multi-family residential, retail commercial, and office development in the City's neighborhood districts, community, regional, and downtown centers as well as along primary transit corridors/boulevards, while at the same time conserving existing neighborhoods and related districts.*

Policy 3.4.1: *Conserve existing stable residential neighborhoods and lower-intensity commercial districts and encourage the majority of new commercial and mixed-use (integrated commercial and residential) development to be located:*

- a. in a network of neighborhood districts, community, regional, and downtown centers,*
- b. in proximity to rail and bus transit stations and corridors, and*
- c. along the City's major boulevards, referred to as districts, centers, and mixed-use boulevards, in accordance with the Framework Long-Range Land Use Diagram.*

Goal 3F: *Mixed-use centers that provide jobs, entertainment, culture, and serve the region.*

Objective 3.10: *Reinforce existing and encourage the development of new regional centers that accommodate a broad range of uses that serve, provide job opportunities, and are accessible to the region, are compatible with adjacent land uses, and are developed to enhance urban lifestyles.*

The Project would support the above goals, objectives, and policies of the Land Use Chapter of the Framework Element through the demolition of a 5,738 square-foot vacant educational building, 8,225 square foot sporting goods store, and surface parking lot for the construction of

over 140,000 square feet of floor area for medical offices and medical care and 5,000 square feet of floor area for commercial uses within a designated Regional Commercial and Transit Priority Area. The Project Site is currently underutilized for its location adjacent to major corridors, transit, and within a Regional Commercial area.

The Project will bring additional capacity for health services to the region's residents in an area well-served by transit. Thereby, making the services more easily accessible to those without automobiles and encouraging others to use other modes of transit which reduces vehicle trips, vehicle miles traveled, and air pollution. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station which is projected to be completed in the Fall of 2024. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site. These rapid bus and local bus service lines include Metro Lines 720, 20, and 105.

Commercial and restaurant uses will be provided on the ground floor of the twelve-story development with an outdoor plaza suitable for outdoor dining. The ground floor uses will help activate the street and improve the pedestrian experience along the major corridors of Wilshire Boulevard and San Vicente Boulevard.

The mixed-use commercial Project is replacing existing commercial buildings on a site that is zoned for commercial uses. Thus, the Project will conserve existing stable residential areas and continue development within commercially-zoned properties within a designated Regional Commercial area.

As such, the Project will preserve commercial land within a Regional Commercial area by bringing a mix of services to an underutilized site well served by transit. Therefore, the Project is consistent with the applicable goals, objectives, and policies in the Land Use Chapter of the Framework Element.

Chapter 5: Urban Form and Neighborhood Design

Goal 5A: *A liveable City for existing and future residents and one that is attractive to future investment. A City of interconnected, diverse neighborhoods that builds on the strengths of those neighborhoods and functions at both the neighborhood and citywide scales.*

Objective 5.2: *Encourage future development in centers and in nodes along corridors that are served by transit and are already functioning as centers for the surrounding neighborhoods, the community or region.*

Policy 5.2.1: *Designate centers and districts in locations where activity is already concentrated and/or where good transit service is, or will be provided.*

Objective 5.5: *Enhance the liveability of all neighborhoods by upgrading the quality of development and improving the quality of the public realm.*

Objective 5.9: *Encourage proper design and effective use of the built environment to help increase personal safety at all times of the day.*

Policy 5.9.2: Encourage mixed-use development which provides for activity and natural surveillance after commercial business hours through the development of ground floor retail uses and sidewalk cafes. Mixed-use should also be enhanced by locating community facilities such as libraries, cultural facilities or police substations, on the ground floor of such building, where feasible.

The mixed-use commercial Project is replacing existing commercial buildings on a site that is zoned for commercial uses. Thus, the Project will continue development within commercially-zoned properties within a designated Regional Commercial area. The Project will bring additional capacity for health services to the region's residents in an area well-served by transit. Thereby, making the services more easily accessible to those without automobiles and encouraging others to use other modes of transit which reduces vehicle trips, vehicle miles traveled, and air pollution. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station which is projected to be completed in the Fall of 2024. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site. These rapid bus and local bus service lines include Metro Lines 720, 20, and 105.

The building elevations utilize a variety of architectural features, building materials, and changes in depth to break up massing and create a consistent architectural design for the development. Glazed glass panels, grey metal paneling, and exposed concrete columns are incorporated across all facades of the project. Terraced balconies that step up vertically from the southern to northern ends of the façade and a two-story balcony at the northern end of the façade break up the massing while adding a distinctive architectural feature. The parking podium will be well-incorporated into the design with a mix of metal and glazed glass paneling which will screen the parking area from the outside.

The ground level streetscape includes landscaping and seating for the potential outdoor dining area that would be located on the corners of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible areas from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area.

The mixed-use nature of the Project will provide continuous activity from the daytime, with the medical offices, into the evening, with the ground-floor commercial uses. Thus, the Project will help increase safety throughout the day with "eyes-on-the street" for many hours of the day, providing overall improved safety and security on the streets surrounding the Project Site.

Thus, the Project would be consistent with the Urban Form and Neighborhood Design Chapter of the Framework Element.

Chapter 7: Economic Development

The Project will be consistent with the relevant goals and objectives of the Framework Element (Chapter 7), including the following:

Goal 7B: A City with land appropriately and sufficiently designated to sustain a robust commercial and industrial base.

Objective 7.2: *Establish a balance of land uses that provides for commercial and industrial development which meets the needs of local residents, sustains economic growth, and assures maximum feasible environmental quality.*

Policy 7.2.2: *Concentrate commercial development entitlements in areas best able to support them, including community and regional centers, transit stations, and mixed-use corridors. This concentration prevents commercial development from encroaching on existing residential neighborhoods.*

Policy 7.2.3: *Encourage new commercial development in proximity to rail and bus transit corridors and stations.*

Policy 7.2.6: *Concentrate office development in regional mixed-use centers, around transit stations, and within community centers.*

As stated earlier, the Project will construct over 140,000 square feet of floor area for medical offices and medical care and 5,000 square feet of floor area for commercial uses adjacent to major corridors, transit, and within a Regional Commercial area. The development will support the region with the addition of high-quality jobs and specialized health care services. The development will be sited on a commercially-zoned property within a designated Regional Commercial area in an area well-served by transit. Thereby, making the services more easily accessible to those without automobiles and encouraging the use of other modes of transit which reduces vehicle trips, vehicle miles traveled, and air pollution. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station which is projected to be completed in the Fall of 2024. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site. These rapid bus and local bus service lines include Metro Lines 720, 20, and 105.

The Project's development of a mixed-use medical and commercial building would strengthen existing nearby commercial development, provide opportunities for new commercial development and services, and strengthen the economic base by expanding market opportunities for existing and new businesses. The Project anticipates a forecasted increase in peak production employment from the current level of 48 employees to 614 employees at Project buildout. This represents an increase of up to 566 employees during peak production periods, not including employees related to the construction of the Project. These commercial activities would be sited in proximity to existing residential and employment centers, on a commercially-zoned property within a designated Regional Commercial area, and in an area well-served by transit.

The Project will bring health services and commercial uses that would sustain economic growth and a robust commercial base in an area where similar uses currently exist and is developing an underutilized site in proximity to existing activity centers and transit. Thus, the Project is consistent with the applicable goals, objectives, and policies of the Economic Development Chapter of the Framework Element.

Mobility Plan 2035

The Mobility Element 2035 (Mobility Element), adopted in September 2016, guides development of a citywide transportation system with the goal of ensuring the efficient movement of people

and goods and recognizes that primary emphasis must be placed on maximizing the efficiency of existing and proposed transportation infrastructure through advanced transportation technology, reduction of vehicle trips, and focused growth in proximity to public transit. The Mobility Plan 2035 includes goals that define the City's high-level mobility priorities and sets forth objectives and policies to establish a citywide strategy to achieve long-term mobility and accessibility within the City of Los Angeles. The Proposed Project would be in conformance with the following objectives and policies of the Mobility Element as described below.

Chapter 2: World Class Infrastructure

Policy 2.3: Recognize walking as a component of every trip and ensure high-quality pedestrian access in all site planning and public right-of-way modifications to provide a safe and comfortable walking environment.

Chapter 3: Access for All Angelenos

Policy 3.1: Recognize all modes of travel, including pedestrian, bicycle, transit, and vehicular modes – including goods movement – as integral components of the City's transportation system.

Policy 3.3: Promote Equitable land use decisions that result in fewer vehicle trips by providing greater proximity and access to jobs, destinations, and other neighborhood services.

Policy 3.5: Support "first-mile, last-mile solutions" such as multi-modal transportation services, organizations, and activities in the areas around transit stations and major bus stops (transit stops) to maximize multi-modal connectivity and access for transit riders.

Policy 3.8: Provide bicyclists with convenient, secure and well-maintained bicycle parking facilities

The Project has considered and will provide access for all modes of travel, including for pedestrians, bicyclists, and transit users. The Site is in a Transit Priority Area which is defined as being within a 0.25 mile of a site containing an existing rail transit station, a ferry terminal served by either a bus or rail transit service, or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station with an anticipated opening date of Fall 2024. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site. These rapid bus and local bus service lines include Metro Lines 720, 20, and 105.

The Transit Priority Area allows Projects to reduce their code-required parking requirement in exchange for additional bicycle parking. Pursuant to LAMC Section 12.21 A.4©, non-residential projects within a TPA may replace up to 30 percent of the required automobile parking spaces, with bicycle parking at a rate of four bicycle parking spaces per vehicle parking space. The Project will provide a total of 716 bicycle parking spaces and 418 vehicle parking spaces. The bicycle parking areas will include secure bicycle storage, lockers, and showers.

Additionally, the Project would include implementation of a TDM program as part of Project Design Feature TRAF-PDF-1. The program includes strategies that encourages employees and visitors of the Project to use alternative modes of transit through the provision of bicycle and pedestrian amenities, promoting alternative transportation modes, supporting carpools and rideshares, and implementing an employee parking management program.

The Project will create an active pedestrian experience along the Project's primary frontage along South San Vicente Boulevard as well as at the intersection of South San Vicente Boulevard, South Sweetzer Avenue, and Wilshire Boulevard. The entrances for the proposed Medical Office Lobby, retail store, and restaurant spaces are located along the South San Vicente Boulevard frontage, as well as the driveways that provide guest vehicular and bicycle valet access. Due to lobby entrance and driveway configuration, the only entrances that are along the façade are those from the retail store and restaurant, the latter of which includes an outdoor dining plaza. The pedestrian experience is enhanced through upgrades to sidewalks, bicycle parking, building lighting around the Project Site, retention of and/or planting of street trees and landscaping. The ground level streetscape includes landscaping and seating for the potential outdoor dining area that would be located on the corners of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible area from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area.

The Project is consistent with the applicable policies of the Mobility Plan as it is located within walking distance of high-quality transit options, includes ample bicycle parking and facilities, and improves the pedestrian experience. Thus, the health services and commercial uses which the Project will provide will be more accessible to those without automobiles and encourage those with cars to use other modes of transit which reduces vehicle trips, vehicle miles traveled, greenhouse gases, and air pollution.

Health and Wellness Element and Air Quality Element

Adopted in March 2015 with a technical update in November 2021, the Plan for a Healthy Los Angeles lays the foundation to create healthier communities for all Angelenos. As the Health and Wellness Element of the General Plan, it provides high-level policy vision, along with measurable objectives and implementation programs, to elevate health as a priority for the City's future growth and development. Through a new focus on public health from the perspective of the built environment and City services, the City of Los Angeles will strive to achieve better health and social equity through its programs, policies, plans, budgeting, and community engagement. The Project is consistent with the following:

Chapter 2: A City Built for Health

Policy 2.2: Promote a healthy built environment by encouraging the design and rehabilitation of buildings and sites for healthy living and working conditions, including promoting enhanced pedestrian-oriented circulation, lighting, attractive and open stairs, healthy building materials and universal accessibility using existing tools, practices, and programs.

Policy 2.3: Strive to eliminate barriers for individuals with permanent and temporary disabilities to access health care and health resources.

Policy 2.6: Work proactively with residents to identify and remove barriers to leverage and repurpose vacant and underutilized spaces as a strategy to improve community health.

Chapter 5: An Environment Where Life Thrives

Policy 5.1: Reduce air pollution from stationary and mobile sources; protect human health and welfare and promote improved respiratory health.

Policy 5.7: Promote land use policies that reduce per capita greenhouse gas emissions, result in improved air quality and decreased air pollution, especially for children, seniors and other susceptible to respiratory diseases.

Air Quality Element

Policy 4.2.3 Ensure that new development is compatible with pedestrians, bicycles, transit, and alternative fuel vehicles.

Policy 5.1.2 Effect a reduction in energy consumption and shift to non-polluting sources of energy in its buildings and operations,

The Project would comply with applicable provisions of the CALGreen Code and the Los Angeles Green Building Code, which will serve to reduce the Project's energy usage. Furthermore, as conditioned and in compliance with Code requirements, a minimum of 30 percent of the total code-required parking spaces will be capable of supporting future electric vehicle supply equipment (EVSE), and 10 percent of the total code-required parking spaces will be equipped with EV chargers. In addition, the Applicant is amenable to providing solar power in the form of either photovoltaic panels or thermal panels, where feasible and approved by the Department of Building and Safety.

The development will be sited on a commercially-zoned property within a designated Regional Commercial area in an area well-served by transit. Thereby, the services would be more easily accessible to those without automobiles and would encourage the use of other modes of transit which reduces vehicle trips, vehicle miles traveled, and air pollution. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station which is projected to be completed in the Fall of 2024. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site. These rapid bus and local bus service lines include Metro Lines 720, 20, and 105.

The pedestrian experience is enhanced through upgrades to sidewalks, bicycle parking, building lighting around the Project Site, retention of and/or planting of street trees and landscaping. The ground level streetscape includes landscaping and seating for the potential outdoor dining area that would be located on the corners of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible area from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area.

The Project's energy efficiency features and location near major transit facilities, which designates it in a Transit Priority Area could help reduce the energy and emission footprint of the Project and the per capita greenhouse gas emissions of the employees and visitors from private automobile travel. The solar-panel roof space and EV-parking are also good zoning practices because they provide a convenient service amenity to the employees or visitors who utilize electricity on site for other functions. As such, the Project provides service amenities and building features to improve the health and air quality for current and future users of the Site. Therefore, the Project would promote a healthy built environment, encourage healthy living and working conditions, reduce air pollution, and promote land use policies that reduce per capita greenhouse gas emissions.

Land Use Element – Wilshire Community Plan

The development of the Project would support the overarching goals of the Wilshire Community Plan. The proposed development furthers the following Community Plan objectives and policies:

Commercial

Objective 2-1: Preserve and strengthen viable commercial development and provide additional opportunities for new commercial development and services within existing commercial areas.

Policy 2-1.1: New commercial uses should be located in existing established commercial areas or shopping centers.

Policy 2-1.2: Protect existing and planned commercially zoned areas especially in Regional Commercial Centers, from encroachment by standalone residential development by adhering to the community plan land use designations.

Objective 2-2: Promote distinctive commercial districts and pedestrian-oriented areas.

Policy 2-2.2: Encourage large mixed use projects to incorporate facilities beneficial to the community such as libraries, child care facilities, community meeting rooms, senior centers, police sub-stations, and/or other appropriate human service facilities as part of the project.

Policy 2-2.3: Encourage the incorporation of retail, restaurant, and other neighborhood serving uses in the first floor street frontage of structures, including mixed use projects located in Neighborhood Districts.

Objective 2-3: Enhance the visual appearance and appeal of commercial districts.

Objective 12-1: Pursue Transportation Demand Management Strategies that maximize vehicle occupancy, minimize average trip length, and reduce the number of vehicle trips.

Policy 12-1.1: Encourage non-residential developments to provide employee incentives for using alternatives to the automobile (car pools, van pools, buses, shuttles, subways, bicycles, walking) and provide flexible work schedules.

Policy 12-1.3: *Require that proposals for major non-residential development projects include submission of a TDM Plan to the City.*

Policy 12-1.4: *Promote the development of transportation facilities and services that encourage higher transit ridership, increased vehicle occupancy, and improved pedestrian and bicycle access.*

Policy 15-1.2: *Develop off-street parking resources, including parking structures and underground parking in accordance with design standards.*

The Project would support the Community Plan's overall goals, objectives, and policies relative to commercial land uses. The Project's development of over 140,000 square feet of floor area for medical offices and 5,000 square feet of floor area for commercial uses would strengthen existing nearby commercial development, provide opportunities for new commercial development and services, and strengthen the economic base by expanding market opportunities for existing and new businesses. The Project anticipates a forecasted increase in peak production employment from the current level of 48 employees to 614 employees at Project buildout. This represents an increase of up to 566 employees during peak production periods, not including employees related to the construction of the Project. These commercial activities would be sited in proximity to existing residential and employment centers, on a commercially-zoned property within a designated Regional Commercial area, and in an area well-served by transit. Thereby, the services would be more easily accessible to those without automobiles and would encourage the use of other modes of transit which reduces vehicle trips, vehicle miles traveled, and air pollution. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station which is projected to be completed in the Fall of 2024. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site.

The pedestrian experience would be enhanced through upgrades to sidewalks, bicycle parking, building lighting around the Project Site, retention of and/or planting of street trees and landscaping. The ground level streetscape includes landscaping and seating for the potential outdoor dining area that would be located on the corners of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. Additionally, the Project would limit driveways to Orange Street and South San Vicente Boulevard, which would reduce potential conflicts between pedestrians and automobiles. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible area from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area.

The Project would include implementation of a TDM program as part of Project Design Feature TRAF-PDF-1. The program includes strategies that encourages employees and visitors of the Project to use alternative modes of transit through the provision of bicycle and pedestrian amenities, promoting alternative transportation modes, supporting carpools and rideshares, and implementing an employee parking management program.

All parking would be provided off-street within the parking podium. The Project is located within a Transit Priority Area which allows Projects to reduce their code-required parking requirement in exchange for additional bicycle parking. Pursuant to LAMC Section 12.21 A.4(c), non-residential projects within a TPA may replace up to 30 percent of the required automobile parking spaces, with bicycle parking at a rate of four bicycle parking spaces per vehicle parking

space. The Project will provide a total of 716 bicycle parking spaces and 418 vehicle parking spaces. The bicycle parking areas will include secure bicycle storage, lockers, and showers. The parking podium is consistent the Citywide Design Guidelines and will include features such as glazed glass panels and metal paneling that shield the parking area and incorporate the podium into the structure through seamless design.

In summary, the Project is consistent with the applicable goals, objectives, and policies of the Wilshire Community Plan though preserving and strengthening commercial areas, adding services beneficial to the community, improving the pedestrian experience, and encouraging alternative modes of travel.

- 2. City Charter Finding 555.** The General Plan may be amended in its entirety, by subject elements or parts of subject elements, or by geographic areas, provided that the part or area involved has significant social, economic or physical identity.

Amendment in Whole or in Part. The requested General Plan Amendment is an Amendment in Part of the Wilshire Community Plan, as it is for the subject site and not the entire Community Plan. The Project's requested amendment from Limited Commercial to Regional Commercial, and concurrent Vesting Zone Change and Height District Change would allow for the proposed mix of medical offices and commercial uses, supporting the City's efforts to provide jobs, services, and commercial uses in established commercial areas in proximity to various modes of transit.

The 0.74 net acre, trapezoid-shaped Project Site is generally bounded by Orange Street to the north, a shared alleyway to the northeast, Sweetzer Avenue to the east, and South San Vicente Boulevard to the southwest. The Site is currently improved with a 5,738 square-foot vacant educational building and an 8,225 square-foot Big 5 Sporting Goods store. Surface parking associated with these uses is located on the southeastern portion of the Project Site, abutting the frontage road of South San Vicente Boulevard, South Sweetzer Avenue, and the alley to the northeast. Additional surface parking is in the middle of the Project Site between the two buildings, and to the rear of the Big 5 Sporting Goods store. The surrounding area is urbanized and surrounded by a mix of land uses that include multi-family residential and commercial, ranging from low-rise to high-rise buildings, which are physically separated from the Project Site by boulevards, streets, and an alley.

The Project proposes to demolish all existing improvements on the Site and construct 140,305 square feet of medical office space, 4,000 square feet of restaurant space, and 1,000 square feet for other commercial uses, such as a pharmacy. The proposed uses would be built within a single, twelve-story building that includes ground floor lobby and commercial space, four levels of podium parking, and seven levels of medical office uses. The proposed uses are compatible with and complement the existing mix of development within the immediate vicinity.

The Project will help the City achieve land use goals of increasing density near transit and existing activity centers. The Project Site is located on a commercially-zone property within a Transit Priority Area, a designated Regional Commercial area and would be located in close proximity to public transit. Public transit service in the vicinity of the Project Site is currently provided by multiple local lines and regional lines via stops within convenient walking distance along Wilshire Boulevard and San Vicente Boulevard. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station which is projected to be completed in the Fall of 2024. In addition, bus transit access is provided along a

number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site. These rapid bus and local bus service lines include Metro Lines 720, 20, and 105.

Furthermore, the Project Site is within an area designated as Regional Commercial in the Framework Element. Regional Centers are considered a focal point of regional commerce, identity and activity and contain a diversity of uses such as corporate and professional offices, residential, retail commercial malls, government buildings, major health facilities, major entertainment and cultural facilities and supporting services. Generally, different types of Regional Commercial uses will fall within the range of floor area ratios from 1.5:1 to 6:1 and are characterized by developments six- to 20-stories (or higher) in height. The proposed General Plan Amendment would enable the construction of a single, twelve-story building that includes ground floor lobby and commercial space, four levels of podium parking, and seven levels of medical office uses. The Project is consistent with and supports the Regional Center as it includes the development of a major health facility with ground floor commercial and restaurant uses. Additionally, consistent with the Framework Element's Regional Centers, the Project proposes a 12-story tower with an FAR of 4.5:1.

Therefore, the General Plan should be amended in part through the Wilshire Community Plan as the Project would contribute to and strengthen an area which has significant social, economic or physical identity.

The proposed General Plan Amendment complies with the procedures as specified in Section 555 of the Charter, including:

- a) **Initiation of Amendments.** In compliance with this sub-section, on the Director of Planning proposed the amendment to the Wilshire Community Plan (General Plan Land Use Element), pursuant to the memo dated January 19, 2017.
- b) **Commission and Mayoral Recommendations.** The noticing and hearing requirements of the General Plan Amendment were satisfied, pursuant to LAMC Sections 12.36 and 12.32.B-D. The hearings were scheduled, duly noticed, and held virtually in conformity with the Governor's Executive Order N-29-20 and as a result of COVID-19 on February 2, 2022 and March 16, 2022. After the Commission recommends approval of an amendment initiated by the Commission or takes action concerning an amendment initiated by the Director or the Council, the Commission shall forward its recommendation to the Mayor. The Mayor shall have 30 days to forward his or her recommendation to the Council regarding the proposed amendment to the General Plan.

This action is further subject to the following sections of Charter Section 555:

- c) **Council Action.** The Council shall conduct a public hearing before taking action on a proposed amendment to the General Plan. If the Council proposes any modification to the amendment approved by the City Planning Commission, that proposed modification shall be referred to the City Planning Commission and the Mayor for their recommendations. The City Planning Commission and the Mayor shall review any modification made by the Council and shall make their recommendation on the modification to the Council. If no modifications are proposed by the Council, or after receipt of the Mayor's and City Planning Commission's recommendations on any

proposed modification, or the expiration of their time to act, the Council shall adopt or reject the proposed amendment by resolution within the time specified by ordinance.

- d) **Votes Necessary for Adoption.** If both the City Planning Commission and the Mayor recommend approval of a proposed amendment, the Council may adopt the amendment by a majority vote. If either the City Planning Commission or the Mayor recommends the disapproval of a proposed amendment, the Council may adopt the amendment only by a two-thirds vote. If both the City Planning Commission and the Mayor recommend the disapproval of a proposed amendment, the Council may adopt the amendment only by a three-fourths vote. If the Council proposes a modification of an amendment, the recommendations of the Commission and the Mayor on the modification shall affect only that modification.

- 3. City Charter Finding 558.** The proposed Amendment to the Wilshire Community Plan will be in conformance with public necessity, convenience, general welfare and represents good zoning practice.

Public Necessity, Convenience, and General Welfare.

The recommended amendment to the Wilshire Community Plan would re-designate the land use from Limited Commercial to Regional Commercial. In conjunction with the requested amendment, the corresponding Vesting Zone and Height District Change from C1-1VL-O to (T)(Q)C2-2D-O would permit development of the Project Site for the construction of a single twelve-story building comprised of 140,305 square feet of medical office space, 4,000 square feet of restaurant/retail space, and 1,000 square feet for other commercial uses, such as a pharmacy. Presently, the Site is underutilized for a parcel within a Regional Center as identified by the General Plan Framework. It is currently improved with a 5,738 square-foot vacant educational building, an 8,225 square-foot Big 5 Sporting Goods store, and a surface parking lot.

The Project's development of over 140,000 square feet of floor area for medical offices and 5,000 square feet of floor area for commercial uses would strengthen existing nearby commercial development, provide opportunities for new commercial development and services, and strengthen the economic base by expanding market opportunities for existing and new businesses. The Project anticipates a forecasted increase in peak production employment from the current level of 48 employees to 614 employees at Project buildout. This represents an increase of up to 566 employees during peak production periods, not including employees related to the construction of the Project. These commercial activities would be located within proximity to existing residential and employment centers, on a commercially-zoned property within a designated Regional Commercial area, and in an area well-served by transit. Thereby, the services would be more easily accessible to those without automobiles and would encourage the use of other modes of transit which reduces vehicle trips, vehicle miles traveled, and air pollution. The Project Site is located approximately 0.25 miles east of the future Metro D (Purple) Line La Cienega/Wilshire Station which is projected to be completed in the Fall of 2024. In addition, bus transit access is provided along a number of Los Angeles County Metropolitan Transit Authority (Metro) and LADOT bus routes, with multiple stops located within 0.25 miles of the Project Site.

The pedestrian experience is enhanced through upgrades to sidewalks, bicycle parking, building lighting around the Project Site, retention of and/or planting of street trees and landscaping. The ground level streetscape includes landscaping and seating for the potential outdoor dining area

that would be located on the corners of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible areas from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area.

In summary, the Project would place healthcare services and commercial uses on an underutilized site within an urbanized commercial area, as well as near public transit and multi-family residential areas. The Project locates needed services near existing employment centers, entertainment, and transit while creating new employment opportunities, office, retail, and restaurants for the neighborhood. The improvement of the streetscape for pedestrian amenities and gathering will also enhance the area. As such, the requested amendments would be in conformity with public necessity, convenience, and general welfare.

Good Zoning Practice

Although the Site is designated as Limited Commercial in the Wilshire Community Plan, the Site is immediately adjacent to Regional Commercial zoned sites to the east along Wilshire Boulevard mainly comprised with high-rise commercial and office buildings, and is located within a Regional Center, as identified by the General Plan Framework. The southern façade of the proposed twelve-story building is on Wilshire Boulevard and would continue a consistent street wall along Wilshire Boulevard. As stated above, the site is currently underutilized with two low-rise structures (one vacant and the other a sporting goods store) and surface parking that fronts Wilshire and San Vicente Boulevards. The proposed structure would bring much needed health care services, and ground floor commercial and restaurant uses to an intersection of major corridors that serve as the western gateway into the city along Wilshire Boulevard. The pedestrian enhancements described earlier along with the ground floor commercial and restaurant spaces will bring vitality to the area and will be safer for pedestrians.

In addition, the Project Site is located within a Transit Priority Area. As previously mentioned, the Project is within an employment center comprised of a mix of uses including office and retail-commercial uses located 0.25 miles east of the future Wilshire Boulevard/La Cienega Boulevard Metro D (Purple) Line Station, which is currently anticipated to be operational in the fall of 2024. The proximity to transit will allow vital health care services to be more accessible to those without a car and encourage others to use forms of transit other than private automobiles which reduces vehicle trips, vehicle miles traveled, greenhouse gases, and air pollution.

As proposed, the Project would bring health care services, commercial uses, and restaurant uses along a commercial corridor with similar uses and building footprints. The proximity to transit will add accessibility and convenience for employees and visitors to the proposed offices, stores, and restaurants. The addition of pedestrian amenities with ground-floor retail and restaurant uses will enliven the street and improve the pedestrian experience. Furthermore, the amendments would allow for the space and height to make this Project feasible. The amendments are consistent with the goals, objectives and policies of the General Plan as stated in Finding Number 1. Therefore, the requested General Plan Amendment, Vesting Zone Change, and Height District Change would represent good zoning practices and development patterns in this portion of the Wilshire Community Plan area.

4. Zone Change, Height District Change, and “T”, “Q”, and “D” Classification Findings.

- a. **Pursuant to Section 12.32 C of the Los Angeles Municipal Code (LAMC), and based on these findings, the recommended action is deemed consistent with the General Plan and is in conformity with public necessity, convenience, general welfare and good zoning practice.**

Public Necessity. The subject property is currently designated and zoned for Limited Commercial land uses and C1 commercial zoning with a Height District 1VL, which limits FAR to 1.5:1 and the height of development to 45 feet. The recommended amendment to the Wilshire Community Plan would re-designate the land use designation of the Project Site from Limited Commercial to Regional Commercial. The recommended Vesting Zone and Height District Change from C1-1VL-O to (T)(Q)C2-2D-O would permit the development of the Project Site with a new medical office building project comprised of 140,305 square feet of medical office space and 5,000 square feet of ground floor retail-commercial space, of which up to 4,000 square feet may be a restaurant and 1,000 square feet may be other commercial uses, such as a pharmacy. As the existing land use designation and zone would not permit the proposed floor area (FAR of 4.5:1) and the height of the Project (230 feet), the amendment and zone change and height district change is necessary to permit the redevelopment of the site.

The Project would be an infill development located within .25 miles of the future Metro D (Purple) Line Wilshire/La Cienega station, providing a mixture of medical office and retail commercial uses. The Project Site is immediately surrounded by low-rise residential buildings and mid- to high-rise commercial and office buildings of varying age from the 1920's to the 1980's. As described by the Community Plan, the area is part of a historic thoroughfare and commercial corridor, Wilshire Boulevard, as well as the South San Vicente Boulevard commercial corridor. The Project Site is at the intersection of these two corridors, which include a variety of medical services such as offices, clinics, small practices, acute care centers, hospitals, etc.

Job creation and retention are a major priority for the City of Los Angeles, as is the new development required to sustain such job growth. The General Plan Framework Element's economic development policies are designed to facilitate job growth by emphasizing that Los Angeles plays a proactive role in the retention and attraction of businesses to have a sufficient job base to maintain and enhance the quality of life. The General Plan Framework Element sets forth a policy to concentrate commercial and office development in centers, corridors, and in proximity to current and planned transit stations.

The Wilshire Community Plan emphasizes many policies to maintain the community's distinctive character, including promotion of multimodal transportation (e.g., walking, bicycling, driving, and taking public transit), improvements to site access and circulation along a central commercial corridor, and the creation of a mobility-friendly environment through active ground floor uses and pedestrian-oriented design. The Project would also provide opportunities for employment of the local workforce in an accessible location within proximity to transit. Further, the Project would encourage the enhancement of the visual environment and provide pedestrian amenities along South San Vicente Boulevard.

The Project anticipates a forecasted increase in peak production employment from the current level of 48 employees to 614 employees at Project buildout. This represents an increase of up to 566 employees during peak production periods, not including employees related to the construction of the Project. The amount of new development required to support this forecasted employment growth is in conformity with public necessity in that it satisfies the intent of both the City's General Plan Framework and the Wilshire Community Plan with regard to job creation and retention.

Accordingly, the proposed Vesting Zone and Height District Change would be in conformity with public necessity.

Convenience. Approval of the Vesting Zone and Height District Change would permit the development of the Project Site with a new medical office building project comprised of 140,305 square feet of medical office space and 5,000 square feet of ground floor commercial space, of which up to 4,000 square feet may be a restaurant and 1,000 square feet may be other commercial uses, such as a pharmacy. The Project is an employment center comprised of a mix of uses including office and retail-commercial uses on a previously developed infill site located 0.25 miles east of the future Wilshire Boulevard/La Cienega Boulevard Metro D (Purple) Line Station, which is currently under construction and is anticipated to be operational in 2024 the same year the Project is anticipated to be completed and occupied. The Project would provide approximately 4,643 square feet of open space areas, which would include new small terraced landscaped patios that would overlook South San Vicente Boulevard, and planting of trees. As proposed, the Project would improve the livability and general welfare of the future patients and employees of the development. The Project would further promote foot traffic through the development of a ground level restaurant or retail uses, street trees and landscaping, and signage and lighting compatible with the surrounding area.

In sum, the Project locates a well-designed commercial and medical office near compatible surrounding medical facilities, housing, and transit. The Project creates a commercial and medical office project within a transit priority area with ground floor restaurant and retail, street trees and lighting that promotes pedestrian activity in the general area. Accordingly, the proposed Zone and Height District Change would be in conformity with the public convenience.

General Welfare. Approval of the requested Vesting Zone Change and Height District Change would allow for the development of a new medical office building project that would support the City's job creation and retention. The Project would create approximately 566 net new jobs on the Project Site, as well as provide medical office space for the neighborhood and region, in addition to neighborhood-serving retail and restaurant uses. Thus, the Project would help to address the City's need for employment opportunities. In addition, the Project would make efficient use of land by adding density while by providing employment opportunities adjacent to public transit, to the benefit of the future on-site employees as well as the existing neighborhood. Accordingly, the zone and height district change would be in conformity with general welfare.

Good Zoning Practice. The Project Site is within the planning boundary of the Wilshire Community Plan area and has a General Plan land use designation of Limited Commercial. The Project Site is zoned C1-1VL-O, which permits commercial and retail uses. With the request for a Vesting Zone and Height District change to (T)(Q)C2-2D-O,

the Project would be consistent with the proposed Regional Center land use designation. The site and adjacent areas along the Wilshire Boulevard corridor are also identified as a Regional Center by the General Plan Framework. Although the Project Site, as well as other properties along South San Vicente, is designated for Limited Commercial land uses, the Project Site is situated at the intersection of South San Vicente Boulevard and Wilshire Boulevard, the latter of which is characterized by mid- to high-rise office, retail, and residential buildings of widely varying age. The Project's vicinity includes various mid- to high-rise office buildings, in addition to low-rise commercial and residential buildings, in the area and along the South San Vicente Boulevard commercial corridor. Properties to the east of the Project Site, along Wilshire Boulevard have a Regional Commercial land use designation and are primarily made up of high-rise office and commercial buildings.

As stated above, the Project would include the construction of a new mixed-use development, comprised of a 12-story mixed-use building with a maximum height of 230 feet. The Project would be consistent with the growth in medical office uses and floor area in and around this neighborhood, which also includes the Cedars Sinai Medical Center.

As proposed, the zone change would result in a Project that increases employment-generating uses on the site that complement and add to existing medical office uses in the vicinity. The Project promotes a more walkable lifestyle by locating commercial uses within proximity of transit and existing residential uses, job centers, and services. Furthermore, the zone change would allow the development of the site with medical and commercial uses that are consistent with the objectives and policies of the Community Plan and are compatible with the existing and proposed development of the surrounding area. Therefore, the zone and height district change would be in conformity with good zoning practices and with development patterns in the immediate area.

In addition, the Project Site is located within a Transit Priority Area (TPA) and a Southern California Association of Governments (SCAG)-designated High Quality Transit Area (HQTa). As previously mentioned, the Project is an employment center comprised of a mix of uses including office and retail-commercial uses on a previously developed infill site located 0.25 miles east of the future Wilshire Boulevard/La Cienega Boulevard Metro D (Purple) Line Station, which is currently under construction and is anticipated to be operational in 2024 the same year the Project is anticipated to be completed and occupied. Specifically, the activated ground floor and landscaping and lighting would support walkability for the Project.

b. "T", "Q", and "D" Classification Findings.

Per LAMC Section 12.32 G.1, 2, and 4, the current action, as recommended, has been made contingent upon compliance with new "T", "Q", and "D" conditions of approval imposed herein for the Project. The "T" Conditions are necessary to ensure the identified dedications, improvements, and actions are undertaken to meet the public's needs, convenience, and general welfare served by the actions required. These actions and improvements will provide the necessary infrastructure to serve the proposed community at this Site. The "Q" and "D" conditions that limit the scale and scope of future development on the Site are also necessary to protect the best interests of and to assure a development more compatible with surrounding properties and the overall pattern of development in the community, to secure an appropriate development in harmony with

the General Plan, and to prevent or mitigate the potential adverse environmental effects of the subject recommended action.

Site Plan Review Findings

5. The project is in substantial conformance with the purposes, intent and provisions of the General Plan, applicable community plan, and any applicable specific plan.

The Project would involve the demolition of an existing 5,738 square-foot, vacant educational building, and an 8,225 square-foot Big 5 Sporting Goods store and associated surface parking to develop a medical office and retail-commercial development on an approximately 0.74-acre (32,290 net square feet) site. The Project would include up to 145,305 square feet of floor area comprised of 140,305 square feet of medical office space and 5,000 square feet of ground floor commercial space, of which up to 4,000 square feet may be a restaurant and 1,000 square feet may be other commercial uses, such as a pharmacy.

The Los Angeles General Plan sets forth goals, objectives and programs that guide both Citywide and community-specific land use policies. The General Plan is comprised of a range of State-mandated elements, including, but not limited to Housing and Conservation, Land Use, Noise, Safety, and Transportation. The City's Land Use Element is divided into 35 Community Plans that establish parameters for land use decisions within those sub-areas of the City. The Project is consistent with the following Elements of the General Plan: Framework Element, Housing Element, Mobility Element, Health and Wellness Element, Air Quality Element, and the Land Use Element- Wilshire Community Plan.

As discussed in Finding No. 1, the Project would be consistent with the purposes, intent and provisions of the General Plan and its elements, including the Framework Element, Housing Element, Mobility Element, Health and Wellness Element and Air Quality Element, and the Land Use Element – Wilshire Community Plan that relate to commercial and economic vitality. Approval of the Project would enhance the built environment in the surrounding neighborhood and would provide a function that is fitting and compatible with the character of the surrounding community and commercial viability of the region as a whole.

Based on the above, the Project is in substantial conformance with the purposes, intent and provisions of the General Plan, applicable community plan, and any applicable specific plan.

6. The project consists of an arrangement of buildings and structures (including height, bulk and setbacks), off-street parking facilities, loading areas, lighting, landscaping, trash collection, and other such pertinent improvements that is or will be compatible with existing and future development in neighboring properties.

The area surrounding the Project Site is highly urbanized, bordered by mid- and high-rise commercial, office, medical-related, multi-family, and single-family residential uses. The Project Site is bound by South San Vicente Boulevard to the southwest where most of the office, medical, and multi-family uses are located, and South Sweetzer Avenue to the east where the office and single-family residential uses are located, and Orange Street to the north where multi-family and single-family residential uses are located. Vehicular access to the Site is currently available from two driveways located along the frontage road of South San Vicente Boulevard and one parking entrance located along Orange Street.

The Project Site is presently developed with a 5,738 square-foot vacant education building and an 8,225 square-foot Big 5 Sporting Goods store. Surface parking associated with these uses is located on the southeastern portion of the Project Site. Additional surface parking is located in the middle of the Project Site between the two buildings, and to the rear of the Big 5 Sporting Goods store. The Project would replace the existing uses on the Project Site with a 12-story medical office/retail-commercial building with up to 145,305 square feet of floor area, resulting in a 4.5:1 floor area ratio (FAR), comprised of up to 140,305 square feet of medical office uses and 5,000 square feet of ground floor commercial uses. The proposed building would be approximately 218 feet in height (230 feet to the top of the mechanical penthouse), with seven floors of medical office uses over four levels of above-grade parking, and a ground floor containing a lobby for the medical office and retail-commercial uses for a total of 12 stories.

The Project would provide 418 parking spaces, including 393 vehicle parking spaces for medical office and 25 vehicle parking spaces for retail-commercial uses. The parking garage would serve as a full-valet garage. The Project would also include 716 bicycle parking spaces for short- and long-term use.

Height

The proposed building would be 230 feet to the top of the mechanical penthouse, with seven floors of medical office uses over four levels of above-grade parking, and a ground floor containing a lobby for the medical office and retail-commercial uses for a total of 12 stories. The proposed medical commercial building would be a similar height as other buildings in the immediate surrounding area. Directly across from the Project Site, is a 10-story office building with ground floor commercial uses. North of the 10-story office building is a three-story office/retail building and two apartment complexes, two- and three-stories in height. To the southeast, fronting Wilshire Boulevard is a 22-story medical office building. Directly east is a two-story office building and a 12-story office building. As such, approval of the Project would allow for the development and use of the Site for medical office and commercial uses consistent with the scale of existing and proposed developments within the surrounding neighborhood.

Bulk & Mass

The area surrounding the Project Site is characterized by a range of one- to 22-story buildings. The proposed Project is for a 12-story building that spans from South San Vicente Boulevard, South Sweetzer Avenue and the alley to the northeast. The building design is intended to be complementary to surrounding structures and reflect contemporary high-rise commercial and office buildings extending along South San Vicente Boulevard and Wilshire Boulevard.

The building is designed as a modern building with stepped terraces and a recessed ground floor to break up the building's massing. The height of the proposed building would visually serve as a horizontal extension of the office buildings to the north and south. The approximately 16 feet tall floor-to-ceiling clear glass panels would bring light and views directly into the medical office spaces. The building is located on the corner of Wilshire Boulevard and South Sweetzer Avenue, where larger buildings line the street. The building steps back toward the shorter office buildings on South San Vicente Boulevard and towards the residential neighborhood to the north. The building's corner edge on San Vicente and Wilshire Boulevard/Sweetzer Avenue is rounded which allows a softening of the overall façade. Grey metal paneling is proposed on the north elevation that would screen the ground level parking. The façade materials palette

consists of glass glazing system, grey metal panels, exposed concrete columns for Levels 2 through 12 and Concrete Masonry Units (CMU) walls added on the ground floor.

The proposed bulk and mass would be consistent with the scale of existing and future proposed developments within the surrounding neighborhood.

Setbacks

Pursuant to LAMC Section 12.13, front yard, side yard and rear yard setbacks are not required in the C2 Zone for commercial uses. As such, the Project proposes zero-front yard, side yard, and rear yard setbacks.

Off-Street Parking and Loading Area

Vehicle access for employees to valet within the parking levels (Floors 2 through 5) would be provided from Orange Street. A visitor drop-off and valet area would be accessible from the frontage road of South San Vicente Boulevard and would accommodate a parking queue and ride-share drop-off area. A loading dock serving the medical office and retail-commercial uses would be located and accessed from Orange Street. Vehicle access to the building's parking garage would not be provided through the alley. The driveway and site access areas would be designed in accordance with the City of Los Angeles Department of Transportation (LADOT) standards. Operation hours for the loading corridor would be likely occur during normal operation hours for the medical offices. In addition, the Project would implement Transportation Demand Management (TDM) and Neighborhood Transportation Management Program (NTMP), to encourage the use of alternate transportation to help reduce traffic amounts in general, as well address potential residential cut-through traffic along Orange Avenue and off-street parking impacts.

The Project would provide 418 vehicle parking spaces within four above-ground levels (Floors 2 through 5) and would include 393 vehicle parking spaces for medical office uses and 25 vehicle parking spaces for ground level retail-commercial uses. Parking spaces on each level would be provided in combination of single, Americans with Disability's Act (ADA), and double-stacked parking spaces, which would require 20-foot ceiling heights for Floors 2 through 5. Pursuant to LAMC 12.21 A.4(c), the combination of medical office and retail-restaurant uses would require a total of 746 vehicle parking spaces. Pursuant to LAMC Section 12.32 P, the Project is requesting a reduction in parking not to exceed 20 percent, incident to a legislative action, reducing the required vehicle parking to a total 597 spaces. As required by LAMC Section 12.21 A. 16, the Project would be required to provide 15 bicycle parking spaces. However, pursuant to LAMC Section 12.21 A.4(c), non-residential projects within a TPA may replace up to 30 percent of the required automobile parking spaces, or a reduction of 179 vehicle parking spaces, with bicycle parking at a rate of four bicycle parking spaces per vehicle parking space, thereby, further reducing the required vehicle parking spaces to 418 spaces, in exchange for providing 716 bicycle parking spaces. The Project would provide a total of 716 bicycle parking spaces and 418 vehicle parking spaces.

Consistent with the requirement of the Los Angeles Green Building Code, the Project would provide 84 parking spaces that would be capable of supporting future electrical vehicle supply equipment (EVSE) and 42 parking spaces that would be equipped with electric vehicle (EV) charging stations. Parking areas would be screened with glass and metal panels and design to blend with the building's architecture to minimize views of the parking uses from the South San Vicente Boulevard frontage.

Landscaping:

The Project is a non-residential medical office and commercial Project, therefore LAMC Section 12.21 G open space requirements do not apply. However, the Project is providing the following landscaping and open space amenities as part of the Project. The ground level streetscape includes landscaping and seating for the potential outdoor dining area that would be located on the corners of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible areas from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area. In addition, Floors 6 through 10 would include small terraced landscaped patios that would overlook South San Vicente Boulevard and would be exclusively accessible by the building tenants. Floor 6 would include 1,864 square feet of landscaped areas, Floor 7 would include 328 square feet of landscaped areas, Floor 8 would include 570 square feet of landscaped area, Floor 9 would include 533 square feet of landscaped area, and Floor 10 would include 533 square feet of landscaped area.

There are seven trees existing on the Project Site, all of which are significant (eight inches or greater of cumulative trunk diameter if multi-trunked, as measured 54 inches above ground), non-protected trees. The Project would replace all significant, non-protected trees at a 1:1 ratio with a minimum of 24-inch box tree. The Project would provide 17 trees on the ground level, with additional trees provided in the small terraced landscaped patios on Floors 6 through 10.

Trash Collection

As conditioned, all trash and recycling areas will be enclosed, accessed via the loading area on Orange Street, and not openly visible from the public right-of-way.

Lighting & Building Signage

New lighting would include building identification, commercial accent lighting, wayfinding, balcony/garden lighting, and security lighting. Pedestrian areas including pathways and entryways into the Project would be well-lit for security and lighting would be ground mounted. As required by LAMC Section 93.0117(b), exterior light sources and building materials would be designed such that they would not cause more than two (2) foot-candles of lighting intensity or generate direct glare onto exterior glazed windows or glass doors on any property containing residential units; an elevated habitable porch, deck, or balcony on any property containing residential units; or any ground surface intended for uses, such as recreation, barbecue or lawn areas, or any other property containing a residential unit or units. Light fixtures would be shielded and directed towards the areas to be lit and away from adjacent light-sensitive residential land uses.

Building identification signage would be provided for the ground level retail and/or restaurant uses. The building would also include street address and identification/wayfinding signage for the vehicular and pedestrian entries to the building. In addition, the building would include directional signage that prioritizes and directs the pedestrian and guest vehicular access to the frontage road of South San Vicente Boulevard and Orange Street. No off-site billboard advertising is proposed as part of the Project. All proposed signage would be designed in conformance with applicable LAMC requirements.

7. That any residential project provides recreational and service amenities in order to improve habitability for the residents and minimize impacts on neighboring properties.

The Project is a non-residential Project and entirely office and commercial use only; however, the Project will provide landscaped open space amenities as detailed below. The ground level streetscape includes landscaping and seating for the potential outdoor dining area that would be located on the corners of South Sweetzer Avenue, Wilshire Boulevard, and South San Vicente Boulevard. The retail and restaurant space on the ground floor of the proposed building would be visible through clear windows and doors to create an inviting and accessible areas from the sidewalk. The ground floor would include 815 square feet dedicated to outdoor seating for the potential café area. In addition, Floors 6 through 10 would include small terraced landscaped patios that would overlook South San Vicente Boulevard and would be exclusively accessible by the building tenants. Floor 6 would include 1,864 square feet of landscaped areas, Floor 7 would include 328 square feet of landscaped areas, Floor 8 would include 570 square feet of landscaped area, Floor 9 would include 533 square feet of landscaped area, and Floor 10 would include 533 square feet of landscaped area.

The Project would include a ground floor pharmacy and restaurant with both indoor and outdoor seating in addition to the medical offices that make up the majority of the Project. Short-term bike parking and a bike valet for both short-term and long-term bike parking would be provided inside of the building near the visitor entrance on the ground level and long-term bike parking would be placed on the rooftop level. Visitors and staff would be able to dine on-site and use medical services during business hours.

As proposed, the Project has programmed the open space taking into consideration the varying recreational needs of the future staff and visitors. Therefore, the Project would provide medical service and commercial and restaurant amenities to improve the habitability for its users and minimize impacts on neighboring properties.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) FINDINGS

The City of Los Angeles (the "City"), as Lead Agency, has evaluated the environmental impacts of the 656 South San Vicente Medical Office Project by preparing an environmental impact report (EIR) ENV-2017-468-EIR (SCH No. 2020010172). The EIR was prepared in compliance with the California Environmental Quality Act of 1970, Public Resources Code Section 21000 et seq. (CEQA) and the California Code of Regulations Title 14, Division 6, Chapter 3 (the "CEQA Guidelines").

The 656 South San Vicente Medical Office Project, consisting of the Draft EIR, Final EIR and Errata, is intended to serve as an informational document for public agency decision-makers and the general public regarding the objectives and impacts of the 656 South San Vicente Medical Office Project (Project), located at 650-676 South San Vicente Boulevard (Project Site). The Project as analyzed in the EIR, proposes up to 145,305 square feet of floor area, comprised of 140,305 square feet of medical office space and 5,000 square feet of ground floor retail commercial space, of which up to 4,000 square feet may be a restaurant and 1,000 square feet may be other commercial uses, such as a pharmacy. The proposed building would include 12 stories and would measure approximately 218 feet in height (230 feet to the top of the mechanical penthouse). The Project would include seven floors of medical office uses over four

floors of above-grade parking, and a ground floor containing a lobby for the medical office, and commercial uses.

The Draft EIR was circulated for a 46-day public comment period beginning on June 17, 2021 and ending on August 2, 2021. A Notice of Completion and Availability (NOC/NOA) was distributed on June 17, 2021, to all property owners within 500 feet of the Project Site and interested parties, which informed them of where they could view the document and how to comment. The Draft EIR was available to the public at the City of Los Angeles, Department of City Planning, and could be accessed and reviewed by members of the public by appointment with the Planning Department. Additionally, due to the circumstances created by the COVID-19 pandemic, copies of the Draft EIR were made available to the public on CD-ROM or in hard copy upon request to the Department of City Planning at the contact information listed on the NOC/NOA. A copy of the document was also posted online at <https://planning.lacity.org>. Notices were filed with the County Clerk on June 17, 2021.

The Final EIR was then distributed on February 2, 2022. The Advisory Agency certified the EIR on May 3, 2022 (Certified EIR) in conjunction with the approval of the Project's Tract Map (VTT-74865), which was subsequently appealed to the City Planning Commission. In its June 23, 2022 meeting, the City Planning Commission voted to deny the appeals and sustain the actions of the Advisory Agency in certifying the EIR. In connection with the certification of the EIR, the City Planning Commission adopted CEQA findings and a Mitigation Monitoring Program. The City Planning Commission adopted the Mitigation Monitoring Program in the EIR as a condition of approval. All mitigation measures in the Mitigation Monitoring Program are also imposed on the Project through Conditions of Approval for the Tract Map and of CPC-2017-467-GPA-VZC-HD-SPR, to mitigate or avoid significant effects of the Project on the environment and to ensure compliance during implementation of the Project.

NO SUPPLEMENTAL OR SUBSEQUENT REVIEW IS REQUIRED

CEQA and the State CEQA Guidelines (California Code of Regulations, Title 14, Chapter 3, Sections 15000-15387) allow the City to rely on the previously certified EIR unless a Subsequent or Supplemental EIR is required. Specifically, CEQA Guidelines Sections 15162 and 15163 require preparation of a Subsequent or Supplemental EIR when an EIR has been previously certified or a negative declaration has previously been adopted and one or more of the following circumstances exist:

- 1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- 2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- 3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:

- A. The project will have one or more significant effects not discussed in the previous EIR or negative declaration;
- B. Significant effects previously examined will be substantially more severe than shown in the previous EIR;
- C. Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

None of the above changes or factors has arisen since the approval of the Project. There are no substantial changes to the Project, and it is substantially the same as the approved project. No substantial changes have been identified to the surrounding circumstances, and no new information of substantial importance has been identified since the approval of the Project. There is no evidence of new or more severe significant impacts, and no new mitigation measures are required for the project.

Accordingly, there is no basis for changing any of the impact conclusions referenced in the certified EIR's CEQA Findings. Similarly, there is no basis for changing any of the mitigation measures referenced in the certified EIR's CEQA Findings, all of which have been implemented as part of the conditions of approval. There is no basis for finding that mitigation measures or alternatives previously rejected as infeasible are instead feasible. There is also no reason to change the determination that the overriding considerations referenced in the certified EIR's CEQA Findings, and each of them considered independently, continue to override the significant and unavoidable impacts of the Project.

Therefore, as the Project was assessed in the previously certified EIR, and pursuant to CEQA Guidelines Section 15162, no supplement or subsequent EIR or subsequent mitigated negative declaration is required, as the whole of the administrative record demonstrates that no major revisions to the EIR are necessary due to the involvement of new significant environmental effects or a substantial increase in the severity of a previously identified significant effect resulting from changes to the project, changes to circumstances, or the existence of new information. In addition, no addendum is required, as no changes or additions to the EIR are necessary pursuant to CEQA Guidelines Section 15164.

RECORD OF PROCEEDINGS

The record of proceedings for the decision includes the Record of Proceedings for the original CEQA Findings, including all items included in the case files, as well as all written and oral information submitted at the hearings on this matter. The documents and other materials that constitute the record of proceedings on which the City of Los Angeles' CEQA Findings are based are located at the Department of City Planning, 221 N. Figueroa Street, Suite 1350, Los Angeles, CA 90021. This information is provided in compliance with CEQA Section 21081.6(a)(2).

In addition, copies of the Draft EIR, Final EIR, and Erratum, are available on the Department of City Planning's website at <https://planning.lacity.org/development-services/eir> (to locate the documents, search for the environmental case number). Due to government facility closures as a result of the COVID-19 crisis, the Draft and Final EIR documents could not be made available at a public library. However, consistent with state emergency orders, the public was notified of an ability to call or email the City for alternative modes to access the documents or to schedule an appointment to review the documents at the City of Los Angeles, Department of City Planning, 221 North Figueroa Street, Suite 1450, Los Angeles, CA 90012, during office hours Monday -Friday, 9:00 a.m. - 4:00 p.m.

PUBLIC HEARING AND COMMUNICATIONS

A Public Hearing was conducted by the Hearing Officer, on behalf of the City Planning Commission, telephonically and virtually via Zoom on February 2, 2022 and March 16, 2022, both at 9:30 AM. In attendance were the Project Applicant's Team and Representative, and several stakeholders and members of the general public.

Public Hearing and Testimony

At the time of the public hearing, on February 2, 2022 and March 16 2022 at 9:30 AM, there were a total of 55 participants, at maximum and with some overlapping, during the meeting including City Planning Staff, the Applicant team, and members of the public. Forty (40) people, combined and with some overlapping, spoke at the hearing, not inclusive of the Applicant team; five(5) people spoke in support of the project; thirty-five (35) people spoke in opposition to the Project. In addition, a total of five (5) written comments were received ahead of the hearings. Support for the project generally focused on the City's critical need for additional medical office space and job density within the area of the future Metro D (Purple) Line station. Opposition to the project centered on the scale of the Project, the individual and public health impacts related to construction and operation, a perceived increase in traffic and congestion related to intensity of uses on the Project Site, the need for additional parking on-site, alley access and associated potential vehicular conflicts, lack of local hire provisions, and the potential of the Project to discourage renting in adjacent properties due to construction activities.

Summary of Public Hearing and Communications

1. Present: There was a total of 55 participants, at maximum and with some overlapping, during the meeting including City Planning Staff, the Applicant team, and members of the public.
2. Public Speakers: Forty (40) people, combined and with some overlapping, spoke at the hearing, not inclusive of the Applicant team; five (5) people spoke in support of the project; thirty-five (35) people spoke in opposition to the Project. In addition, a total of five (5) written comments were received ahead of the hearings.
3. The Applicant's Representative described the Project design and entitlement requests.
4. Public Hearing Testimony

Speaker Comments Supporting the Project

- The Project will increase LA's much needed medical office and laboratory space, especially after the COVID-19 pandemic, and would free up other space for more acute medical care.
- The Project would support union labor in the building and construction trades.
- The Project will create more density and jobs where there currently is none and will remove a parking lot and auto-oriented commercial use simultaneously.
- Existing problems related to parking and traffic will not be solved with one Project, and therefore should not be considered as a potential reason to deny the Project.

- Project density is needed for LA to build up and not out, in order to decrease car use and sprawl in the region.
- The Project's location near future public transit will be a positive impact for the city and region.

Speaker Comments in Opposition to the Project

- The Project is too large and out of scale for the neighborhood and adjacent residential units.
- Parking will be negatively impacted in the surrounding area because of the parking reductions proposed by the Project.
- Fire response and flow are inadequate.
- The Project should improve the adjacent intersection at South San Vicente Boulevard and Wilshire Boulevard as it is unsafe and hard for pedestrians to use and should care for the median along South San Vicente Boulevard.
- The Project will exacerbate traffic congestion in the area.
- The Project does not propose to use native plantings.
- The Project will increase glare and light pollution in surrounding areas, namely in the Carthay Circle HPOZ.
- Generators, trash, and hazardous materials and waste will be stored adjacent to residential areas.
- Construction will exacerbate local air pollution.
- Construction will negatively impact the public health of those living nearby.
- The Project is not proposing enough community benefits.
- The Project will cause damage to adjacent residential buildings.
- The Project Site should be used for affordable housing, especially at this scale and density.
- The Project does not propose a local hire provision.
- The EIR is insufficient and does not address all environmental impacts.
- Developer was negligent during the development process and has allowed numerous unhoused encampments to create disturbances, including fires and harassment, within the neighborhood.

Response to Public Testimony: The Applicant's Representative clarified statements made regarding the request for housing on the site, the potential impacts to lighting and glare in

the surrounding area, the location and impact of generators, trash, and hazardous materials, the request for construction trade agreements and local hire provisions, the actions the Developer took to address conflicts with unhoused encampments on the site, and the request for additional parking.



656 SOUTH SAN VICENTE MEDICAL OFFICE PROJECT

ENTITLEMENT DRAWINGS JULY 02, 2020
650 SSV PROPERTY OWNER, LLC AND 656-676 SSV PROPERTY OWNER, LLC

rdc. | studioneleven

ZGF
ZIMMER GUNDEL FRASCA ARCHITECTS LLP

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PROJECT INFORMATION

SITE ADDRESSES
650-676 SOUTH SAN VICENTE BOULEVARD
LOS ANGELES, CALIFORNIA 90048

656 SOUTH SAN VICENTE MEDICAL OFFICE PROJECT
145,305 SF MEDICAL OFFICE BUILDING
GROUND FLOOR COMMERCIAL SPACE (RETAIL, RESTAURANT)

ACCESSOR PARCEL NUMBERS
5510-022-033, 5510-022-034, 55100-22-035, 55100-22-058, 55100-22-059

ZONING AND GENERAL PLAN LAND USE DESIGNATION
EXISTING C1-1VL-0 LIMITED COMMERCIAL
PROPOSED (Q) C2-2D REGIONAL CENTER COMMERCIAL

LOT AREA AND BUILDABLE AREA
TOTAL EXISTING LOT AREA 33,060 SF (0.76 ACRE)
BUILDABLE AREA (EXCLUDES DEDICATION AREA) 32,290 SF

LEGAL DESCRIPTION

(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-797214-LA2 DATED JUNE 3, 2016)

THE LAND REFERRED TO IN THIS COMMITMENT IS SITUATED THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATEOF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

LOTS 3, 4, 5 AND 6, BLOCK 4, TRACT 7555, AS PER MAP RECORDED IN BOOK 80, PAGES 51 TO 53, INCLUSIVE, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 5510-022-058 (AFFECTS: LOTS 5 AND 6) AND 5510-022-059 (AFFECTS: LOTS 3 AND 4)

AND

(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-811256-LA2 DATED OCTOBER 13, 2016)

LOTS 7, 8 AND 9 OF BLOCK 4, TRACT NO. 7555, IN THE CITY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 80 PAGES 51 TO 53 INCLUSIVE OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 5510-022-023 (AFFECTS LOT 7) 5510-022-034 (AFFECTS LOT 8) 5510-022-035 (AFFECTS LOTS 9)

HEIGHT

HEIGHT LIMIT C1-1VL-0 (EXISTING ZONING)	45 FT
HEIGHT PROPOSED (Q) C2-2D (PROPOSED ZONING)	230 FT

TREES

TREES REQUIRED C1-1VL-0 (EXISTING ZONING)	0 TREES
TREES PROPOSED (Q) C2-2D (PROPOSED ZONING)	16 TREES

PROJECT DESCRIPTION

THE 656 SAN VICENTE MEDICAL OFFICE PROJECT (PROJECT) WOULD DEMOLISH A 6,067 SQUARE-FOOT EDUCATIONAL BUILDING (WHICH IS CURRENTLY VACANT), AND 5,738 SQUARE-FOOT COMMERCIAL BUILDING, AND ASSOCIATED SURFACE PARKING IN ORDER TO DEVELOP A MEDICAL OFFICE AND RETAIL COMMERCIAL DEVELOPMENT ON AN APPROXIMATELY 0.76-ACRE (33,060 GROSS SQUARE FEET) SITE LOCATED AT 650-676 SOUTH SAN VINCENTE BOULEVARD (PROJECT SITE). THE PROJECT WOULD INCLUDE 145,305 SQUARE FEET OF FLOOR AREA, FOR A 4.5:1 FLOOR AREA RATIO (FAR), THAT WOULD INCLUDE 140,305 SQUARE FEET OF MEDICAL OFFICE SPACE AND 5,000 SQUARE FEET OF GROUND FLOOR COMMERCIAL SPACE. THE PROPOSED BUILDING WOULD BE 12 STORIES AND APPROXIMATELY 218 FEET IN HEIGHT (230 FEET TO THE TOP OF THE MECHANICAL PENTHOUSE). THE PROJECT WOULD PROVIDE 418 PARKING SPACES WITHIN FOUR SCREENED, ABOVE GROUND LEVELS, INCLUDING 393 VEHICLE PARKING SPACES FOR MEDICAL OFFICE AND 24 VEHICLE PARKING SPACES FOR COMMERCIAL USES. THE PROJECT WOULD ALSO INCLUDE 716 BICYCLE PARKING SPACES.

PARKING SPACES ON EACH LEVEL WOULD BE PROVIDED IN A COMBINATION OF SINGLE, AMERICANS WITH DISABILITIES ACT (ADA), AND STACKED PARKING SPACES, WHICH WOULD REQUIRE 20-FOOT CEILING HEIGHTS FOR FLOORS 2 THROUGH 5. PURSUANT TO LAMC SECTION 12.21A.4(C), THE COMBINATION OF MEDICAL OFFICE AND RETAIL-RESTAURANT USES WOULD REQUIRE A TOTAL OF 746 VEHICLE PARKING SPACES, PURSUANT TO LAMC SECTION 12.32 P. THE PROJECT IS REQUESTING A REDUCTION IN PARKING NOT TO EXCEED 20 PERCENT, INCIDENT TO A LEGISLATIVE ACTION, REDUCING THE REQUIRED VEHICLE PARKING TO A TOTAL OF 596 SPACES. AS REQUIRED BY LAMC SECTION 12.21A.16, THE PROJECT WOULD BE REQUIRED TO PROVIDE 15 BICYCLE PARKING SPACES. HOWEVER, PURSUANT TO LAMC SECTION 12.21 A.4(C), NON RESIDENTIAL PROJECTS WITHIN A TPA MAY REPLACE UP TO 30 PERCENT OF THE REQUIRED AUTOMOBILE PARKING SPACES, OR A REDUCTION OF 179 VEHICLE PARKING SPACES, WITH BICYCLE PARKING AT A RATE OF FOUR BICYCLE PARKING SPACES PER VEHICLE PARKING SPACE, THEREBY FURTHER REDUCING THE REQUIRED VEHICLE PARKING SPACED BY 179 SPACES. AS SUCH, THE PROJECT WOULD PROVIDE A TOTAL OF 716 BICYCLE PARKING SPACES AND 418 VEHICLE PARKING SPACES. PARKING FLOORS TO BE SCREENED ON ALL SIDES.

SETBACKS

<u>SETBACKS REQUIRED C1-1VL-0 (EXISTING ZONING)</u>			
FRONT YARD	SOUTH SAN VICENTE BOULEVARD	0 FT	
REAR YARD	ALLEY	0 FT	
SIDE YARD	SOUTH SWEETZER AVENUE	0 FT	
SIDE YARD	ORANGE STREET	0 FT	
<u>SETBACKS PROPOSED (Q) C2-2D (PROPOSED ZONING)</u>			
FRONT YARD	SOUTH SAN VICENTE BOULEVARD	0 FT	
REAR YARD	ALLEY	0 FT	
SIDE YARD	SOUTH SWEETZER AVENUE	0 FT	
SIDE YARD	ORANGE STREET	0 FT	



WILSHIRE AT SAN VICENTE SITE
LOCATED ON THE NORTHEAST CORNER OF SAN VICENTE BLVD. AND WILSHIRE BLVD.

CONTACT INFORMATION

<u>CLIENT</u> 650 SSV PROPERTY OWNER, LLC 656-676 SSV PROPERTY OWNER, LLC CONTACT: BRYAN FAIRBANKS 10850 WILSHIRE BLVD, SUITE 1050 LOS ANGELES, CA90024 P: (310)693-4400 M: (626)348-3712	<u>ARCHITECT</u> RDC. CONTACT: DEREK HEEB 245 EAST THIRD STREET LONG BEACH, CA 90802 P: (562)628-8000 E: DEREK.HEEB@RDCCOLLABORATIVE.COM
	<u>LANDSCAPE ARCHITECT</u> STUDIONELEVEN CONTACT: KIRK KELLER 245 EAST THIRD STREET LONG BEACH, CA 90802 P: (562)901-1500 E: KIRK.KELLER@STUDIO-111.COM

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FLOOR AREA

ALLOWABLE BASE FLOOR AREA

C1-1VL-0 (EXISTING ZONING)	
FAR 1.5:1 (BUILDABLE AREA 32,290 SF x 1.5)	48,435 SF

PROPOSED FLOOR AREA

(Q) C2-2D (PROPOSED ZONING)	
FAR 4.5:1 (BUILDABLE AREA 32,290 SF x 4.5)	145,305 SF

LAMC 13.02 DEFINIITION - FLOOR AREA. (AMENDED BY ORD. NO. 182,386, EFF. 3/13/13.)
THE AREA IN SQUARE FEET CONFINED WITHIN THE EXTERIOR WALLS OF A BUILDING, BUT NOT IN-
CLUDING THE AREA OF THE FOLLOWING: EXTERIOR WALLS, STAIRWAYS, SHAFTS, ROOMS HOUSING
BUILDING-OPERATING EQUIPMENT OR MACHINERY, PARKING AREAS WITH ASSOCIATED DRIVEWAYS
AND RAMPS, SPACE DEDICATED TO BICYCLE PARKING, SPACE FOR THE LANDING AND STORAGE OF
HELICOPTERS, AND BASEMENT STORAGE AREAS.

LEVEL	MEDICAL OFFICE	RETAIL	RESTAURANT	UTILITY (excluded)	PARKING (excluded)
LEVEL 12	6,200 SF	--	--	2,085 SF	--
LEVEL 11	9,600 SF	--	--	2,085 SF	--
LEVEL 10	13,000 SF	--	--	2,085 SF	--
LEVEL 9	27,000 SF	--	--	2,085 SF	--
LEVEL 8	27,000 SF	--	--	2,085 SF	--
LEVEL 7	28,000 SF	--	--	2,085 SF	--
LEVEL 6	28,000 SF	--	--	2,085 SF	--
LEVEL 5	--	--	--	--	32,410 SF
LEVEL 4	--	--	--	--	32,410 SF
LEVEL 3	--	--	--	--	32,410 SF
LEVEL 2	--	--	--	--	32,410 SF
GROUND LEVEL	1,505 SF	1,000 SF	4,000 SF	11,650 SF	-- SF
SUBTOTAL	140,305 SF	1,000 SF	4,000 SF	26,245 SF	129,640 SF

FLOOR AREA SUMMARY (RETAIL, RESTAURANT + MEDICAL OFFICE) 145,305 SF

RESTAURANT OCCUPANCY

A2 AREA	70% DINING AREA*	OCC.LOAD	30% BOH AREA*	OCC. LOAD	TOTAL OCCUPANTS
4,000 SF	2,800 SF	2,800 SF/15 = 186 OCC.	1,200 SF	1,200 SF/300 = 4 OCC.	190 MAX. OCCUPANTS INDOOR AND OUTDOOR

*70/30 AREA RATIO AS INDUSTRY STANDARD, EXACT AREAS AND TENANT IMPROVEMENT TO BE DETERMINED

OPEN SPACE

OPEN SPACE REQUIRED C1-1VL-0 (EXISTING ZONING)	0 SF
--	------

OPEN SPACE PROPOSED (Q) C2-2D (PROPOSED ZONING)

LEVEL 10	533 SF
LEVEL 9	533 SF
LEEVL 8	570 SF
LEVEL 7	328 SF
LEVEL 6	1,864 SF
GROUND LEVEL	815 SF
TOTAL OPEN SPACE	4,643 SF

VEHICLE PARKING

PARKING REQUIRED

USE	AREA	PARKING RATIO REQUIRED (stalls per 1,000 SF)	STALLS REQUIRED	STALLS AFTER 20% REDUCTION*	STALLS AFTER FURTHER 30% REDUCTION**
MEDICAL OFFICE	140,305 SF	5	702	561	393
RETAIL	1,000 SF	4	4	3	2
RESTAURANT	4,000 SF	10	40	32	22
	145,305	5.1	746	596	417
TOTAL STALLS REQUIRED					

* City's Discretionary Parking Reduction for Commercial Projects
** Transit Priority Area Reduction (within 1,500 ft of a transit station)

PARKING PROVIDED

LEVEL	PRIME STANDARD	PRIME SINGLE	ADA STANDARD	ADA VAN	TANDEM OR STACKED TANDEM	SUBTOTAL
05	42 STALLS	2 STALLS	--	--	66 STALLS	110 STALLS
04	44 STALLS	2 STALLS	--	--	68 STALLS	114 STALLS
03	44 STALLS	2 STALLS	--	--	68 STALLS	114 STALLS
02	27 STALLS	3 STALLS	9 STALLS	2 STALLS	39 STALLS	80 STALLS
	157 STALLS	9 STALLS	9 STALLS	2 STALLS	241 STALLS	418 STALLS TOTAL PROVIDED

AS REQUIRED BY CITY PLANNING COMMISSION EV PARKING POLICY AND LAMC SECTION 99.04.106, DIVISION 4, ARTICLE 9,
CHAPTER IX (ORDINANCE NO. 181,480), THE PROJECT WOULD PROVIDE 84 PARKING SPACES THAT WOULD BE CAPABLE OF
SUPPORTING FUTURE ELECTRICAL VEHICLE SUPPLY EQUIPMENT (EVSE) AND 21 PARKING SPACES THAT WOULD BE EQUIPPED
WITH ELECTRIC VEHICLE (EV) CHARGING STATIONS

BICYCLE PARKING

STALLS AFTER 20% REDUCTION*	STALLS AFTER FURTHER 30% REDUCTION*	STALLS REPLACED WITH BICYCLE	BICYCLE TO CAR RATIO	BICYCLES REQUIRED
596	417	179	x4	716

BICYCLE PARKING REQUIRED (City of Los Angeles Ordinance No. 185480)

MEDICAL OFFICE				short term	long term
short term	140,305 SF	x	(1 bicycle per 10,000 SF; 2min)=	14 BICYCLES	--
long term	140,305 SF	x	(1 bicycle per 5,000 SF; 2min)=	--	28 BICYCLES
RETAIL					
short term	1,000 SF	x	(1 bicycle per 10,000 SF; 2min)=	2 BICYCLES	--
long term	1,000 SF	x	(1 bicycle per 10,000 SF; 2min)=	--	2 BICYCLES
RESTAURANT					
short term	4,000 SF	x	(1 bicycle per 2,000 SF; 2min)=	2 BICYCLES	--
long term	4,000 SF	x	(1 bicycle per 2,000 SF; 2min)=	--	2 BICYCLES
				18 BICYCLES ground level	32 BICYCLES roof level

BICYCLE PARKING PROVIDED

ROOF LEVEL	698 BICYCLES flexible long or short term
GROUND LEVEL	18 BICYCLES short term
	716 TOTAL BICYCLES PROVIDED



June 23, 2022

DESIGN SURVEY

COMMENTS

BOUNDARY LINES . . . WERE ESTABLISHED FROM THE RECOVERED CITY, COUNTY AND/OR PRIVATE ENGINEER MONUMENTS WHOSE CHARACTER AND SOURCE ARE SO NOTED ON THE SURVEY.

INDICATES PRELIMINARY TITLE REPORT EXCEPTION NUMBER PLOTTED HEREON

SITE ADDRESS . . . 650 - 676 SOUTH SAN VICENTE, LOS ANGELES, CA 90048

APN NO. 5510-022-033, 034, 035, 058 AND 059.

DATE OF SURVEY . . . JULY 19-20, 2017

LAND AREA 33,060 SQ. FT. OR 0.759 ACRES, MORE OR LESS

FLOOD INSURANCE RATE MAP ZONE "X" AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN PER FLOOD INSURANCE RATE MAP (FIRM) MAP PANEL MAP NO. 06037C1605F EFFECTIVE DATE SEPTEMBER 26, 2008.

UTILITIES ALL OBSERVED UTILITIES SHOWN ON THIS MAP WERE OBTAINED BY CONVENTIONAL MEANS AND RECORD DRAWINGS PER LOS ANGELES NAVIGATE LA WEBSITE. THEY ARE FOR INFORMATION ONLY. NO REPRESENTATION IS MADE AS TO THE COMPLETENESS OF SAID INFORMATION AND ANY USER OF THIS INFORMATION SHOULD CONTACT THE UTILITY OR GOVERNMENT AGENCY DIRECTLY.

PUBLIC ACCESS . . . PUBLIC ACCESS TO SITE FROM SAN VICENTE BOULEVARD, SWEETZER AVENUE, ORANGE AVENUE AND ALLEY.

BASIS OF BEARINGS

THE BEARING OF N 07°49'20" E ALONG THE WESTERLY SIDELINE OF SWEETZER AVENUE AS SHOWN ON THE MAP OF TRACT NO. 7555 FILED IN BOOK 80, PAGES 51 THROUGH 53 INCLUSIVE, OF MAPS, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS MAP.

BENCHMARK INFORMATION

ELEVATIONS SHOWN HEREON ARE RELATIVE TO THE FOLLOWING BENCHMARK:

CITY OF LOS ANGELES BENCHMARK NO. 13-12250
CUT SPK. 8FT N OF N. PL. WILSHIRE BLVD.; 3.5 FT E/O E CURB LINE SWEETZER AVE;
NE COR. CB 50 FT E/O PL. SAN VICENTE BLVD

ELEV. = 141.712 FEET; DATUM: NAVD 1988; YEAR OF ADJUSTMENT: 2000

ZONING

EXISTING: C1-TL-0
PROPOSED: [O]C2-2D

LEGAL DESCRIPTION

(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-797214-LA2 DATED JUNE 3, 2016)

THE LAND REFERRED TO IN THIS COMMITMENT IS SITUATED THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

LOTS 3, 4, 5 AND 6, BLOCK 4, TRACT 7555, AS PER MAP RECORDED IN BOOK 80, PAGES 51 TO 53, INCLUSIVE, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APNs: 5510-022-058 (AFFECTS: LOTS 5 AND 6) AND 5510-022-059 (AFFECTS: LOTS 3 AND 4)

AND

(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-811256-LA2 DATED OCTOBER 13, 2016)

LOTS 7, 8 AND 9 OF BLOCK 4, TRACT NO. 7555, IN THE CITY OF LOS ANGELES, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 80 PAGES 51 TO 53 INCLUSIVE OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APNs: 5510-022-033 (AFFECTS LOT 7) 5510-022-034 (AFFECTS LOT 8) 5510-022-035 (AFFECTS LOT 9)

EXCEPTIONS

(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-797214-LA2 DATED JUNE 3, 2016 AND AMENDED JUNE 20, 2016)

AN EASEMENT FOR POLE LINES AND STORM DRAINS AND INCIDENTAL PURPOSES, RECORDED IN BOOK 3932, PAGE 211 OF OFFICIAL RECORDS.

IN FAVOR OF: CALIFORNIA TRUST COMPANY
AFFECTS: LOT 4, AS DESCRIBED THEREIN

AN EASEMENT FOR POLE LINES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 15, 1950 IN BOOK 3994, PAGE 232 OF OFFICIAL RECORDS.

IN FAVOR OF: CALIFORNIA TRUST COMPANY
AFFECTS: LOT 3, AS DESCRIBED THEREIN

AN EASEMENT FOR POLE LINES AND STORM DRAINS AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 15, 1950 IN BOOK 4486, PAGE 76 OF OFFICIAL RECORDS.

IN FAVOR OF: CALIFORNIA TRUST COMPANY
AFFECTS: LOT 5, AS DESCRIBED THEREIN

AN EASEMENT FOR POLE LINES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 15, 1950 IN BOOK 4884, PAGE 345 OF OFFICIAL RECORDS.

IN FAVOR OF: CALIFORNIA TRUST COMPANY
AFFECTS: LOT 6, AS DESCRIBED THEREIN

AN EASEMENT FOR STORM DRAIN AND INCIDENTAL PURPOSES, RECORDED IN BOOK 10246, PAGE 120 OF OFFICIAL RECORDS.

IN FAVOR OF: CITY OF LOS ANGELES
AFFECTS: LOT 4, AS DESCRIBED THEREIN

AN EASEMENT FOR STORM DRAIN AND INCIDENTAL PURPOSES, RECORDED IN BOOK 10307, PAGE 130 OF OFFICIAL RECORDS.

IN FAVOR OF: CITY OF LOS ANGELES
AFFECTS: LOT 5, AS DESCRIBED THEREIN

AN EASEMENT FOR STORM DRAIN AND INCIDENTAL PURPOSES, RECORDED IN BOOK 10348, PAGE 264 OF OFFICIAL RECORDS.

IN FAVOR OF: CITY OF LOS ANGELES
AFFECTS: LOT 5, AS DESCRIBED THEREIN

EXCEPTIONS

(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-811256-LA2 DATED OCTOBER 13, 2016)

COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED AS BOOK 3055 PAGE 304 OF OFFICIAL RECORDS.

AN EASEMENT AS CONTAINED IN THE ABOVE DOCUMENT, FOR: POLE LINES AND INCIDENTAL PURPOSES. (AFFECTS LOT 8)

COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED AS BOOK 3129 PAGE 355 OF OFFICIAL RECORDS.

AN EASEMENT AS CONTAINED IN THE ABOVE DOCUMENT, FOR: POLE LINES AND INCIDENTAL PURPOSES. (AFFECTS LOT 9)

AN EASEMENT FOR POLE LINES AND INCIDENTAL PURPOSES IN THE DOCUMENT RECORDED DECEMBER 17, 1955 AS BOOK 4531 PAGE 329 OF OFFICIAL RECORDS. (AFFECTS LOT 7)

NOTE

- EXISTING TOPOGRAPHIC INFORMATION SHOWN IN GRAYS SCALE PER PRIOR TOPOGRAPHIC SURVEY PROVIDED BY CLIENT. NO REPRESENTATION IS MADE AS TO THE COMPLETENESS OR ACCURACY OF SAID INFORMATION.
- OTHER TOPOGRAPHIC INFORMATION SHOWN HEREON IS PER KPFF FIELD SURVEY PERFORMED ON 7/19/2017 THROUGH 7/20/2017. KPFF ASSUMES RESPONSIBILITY FOR FIELD INFORMATION SHOWN HEREON ONLY.

PREPARED UNDER THE DIRECTION OF:

CHRISTOPHER JONES, PLS. 8193
CHRIS.JONES@KPFF.COM



EXHIBIT "A"

Page No. 5 of 27

Case No. CPC-2017-467-GPA-VZC-HD-SPR

June 23, 2022

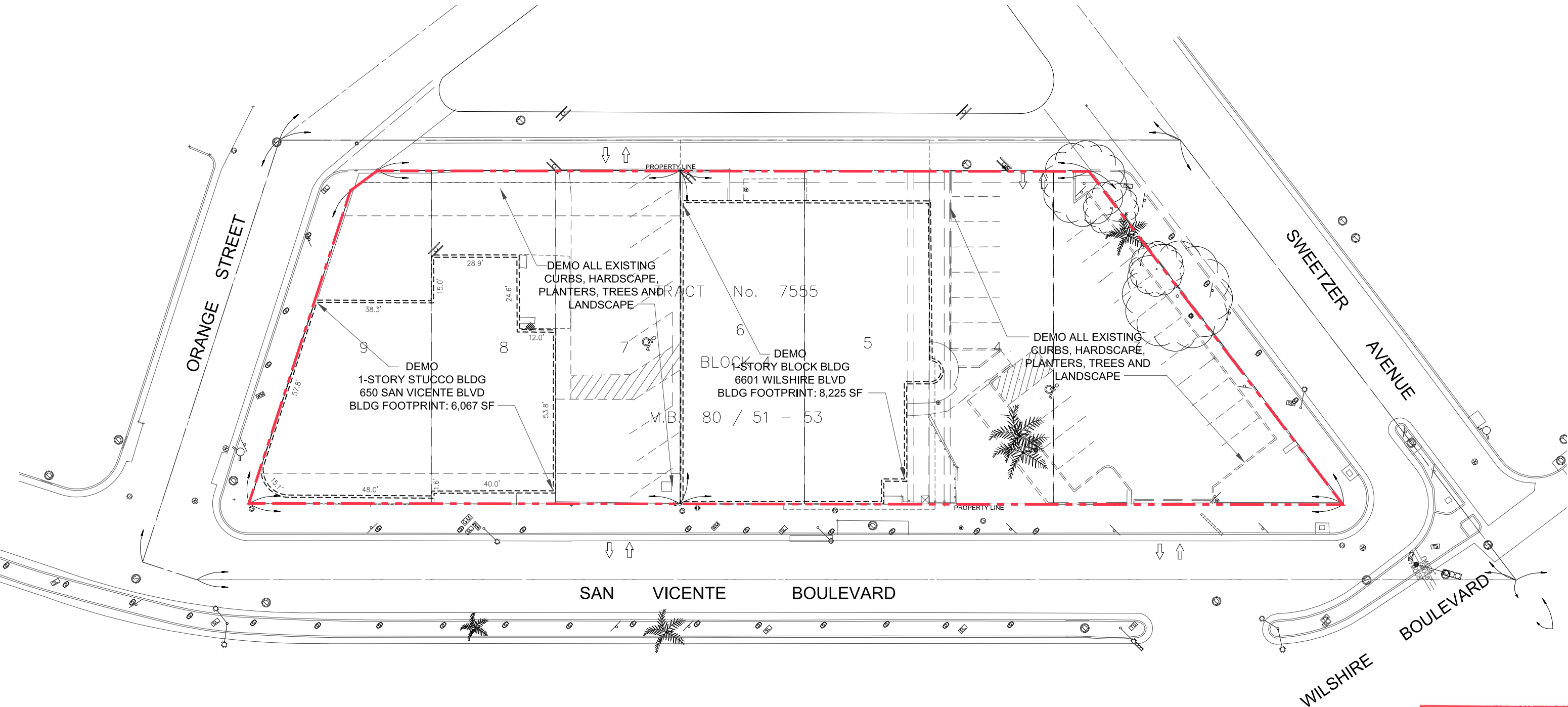


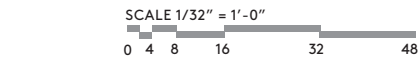
EXHIBIT "A"
Page No. 6 of 27
Case No. CPC-2017-467-GPA-VZC-HD-SPR

June 23, 2022

--- PROPERTY LINE

--- BUILDING FOOTPRINT TO BE DEMOLISHED

SCALE 1/32" = 1'-0"
0 4 8 16 32 48



TREE LEGEND

- Street Tree along San Vicente Blvd.
Platanus acerfolia - London Plane Tree
- Street Tree along Orange Street and Sweetzer
Bauhenia purpurea - Purple Orchid Tree

Note: All planting areas to be irrigated with weather-sensing and automated water efficient drip irrigation system.

Note: See page A.4 for open space plan and table calculations

Landscape and Hardscape Areas

	Hardscape (SF)	Planted Area (SF)
Level 10	388	148
Level 9	386	145
Level 8	386	145
Level 7	234	94
Level 6	1,465	380
Ground Level	19,756	585
Total Areas	22,615	1,497

EXHIBIT "A"

Page No. 7 of 27

Case No. CPC-2017-467-GPA-VZC-HD-SPR

June 23, 2022

KEYNOTES:

- Concrete sidewalk paving with gridded score pattern
- Concrete pavers
- Concrete paving w/ rectangular score pattern
- Planting area, typical
- 4'x8' tree wells along streetscape
- Planter-pots at facade
- Bollards at curb
- Bicycle parking area at groundlevel
- Shade gardens at valet drop-off
- Drought-tolerant garden at retail patio
- Project signage
- Zero curb w/ truncated domes and bollards

SELECTED / PARTIAL PLANT PALETTE



London Plane Tree
Size: 36" Box Min.
Quantity: 9



Purple Orchid Tree
Size: 36" Box Min.
Quantity: 7



Philodendron Xanadu
Size: 5 Gallon Spaced at 24" O.C.



Foxtail Fern
Size: 5 Gallon Spaced at 24" O.C.



Cape Rush
Size: 5 Gallon Spaced at 30" O.C.



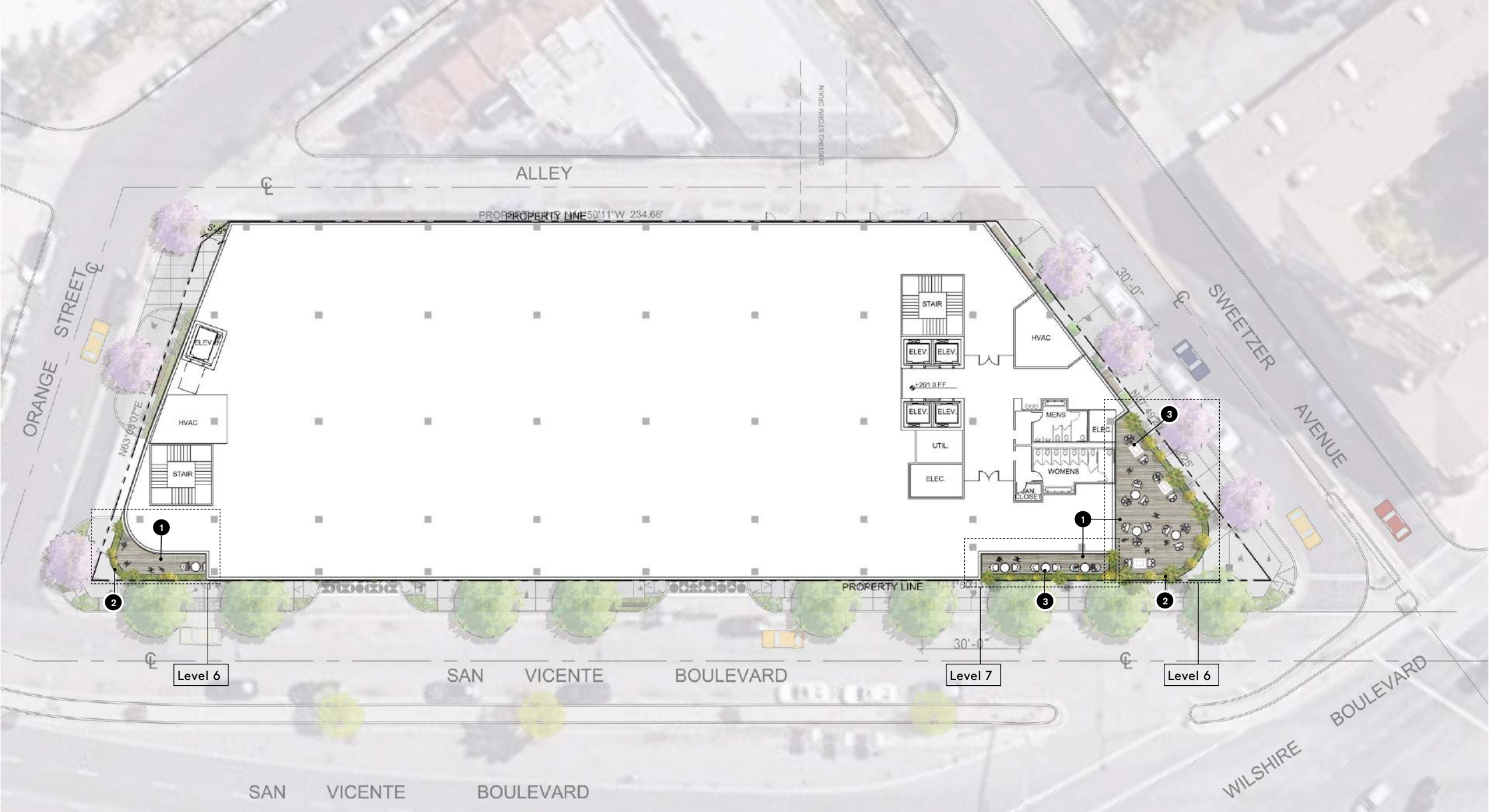
Blue Fescue
Size: 1 Gallon Spaced at 12" O.C.



Slender Veldt Grass
Size: 5 Gallon Spaced at 18" O.C.



Black Dwarf Mondo Grass
Size: 1 Gallon Spaced at 12" O.C.



- KEYNOTES:**
- 1. Wood decking over structure
 - 2. Raised planters w/ drought-tolerant plants
 - 3. Loose seating

Note: All planting areas to be irrigated with weather-sensing and automated water efficient drip irrigation system.

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Case No. CPC-2017-467-GPA-VZC-HD-SPR

June 23, 2022



Oriental Fountain Grass
Size: 5 gallon Spaced at 24" O.C.



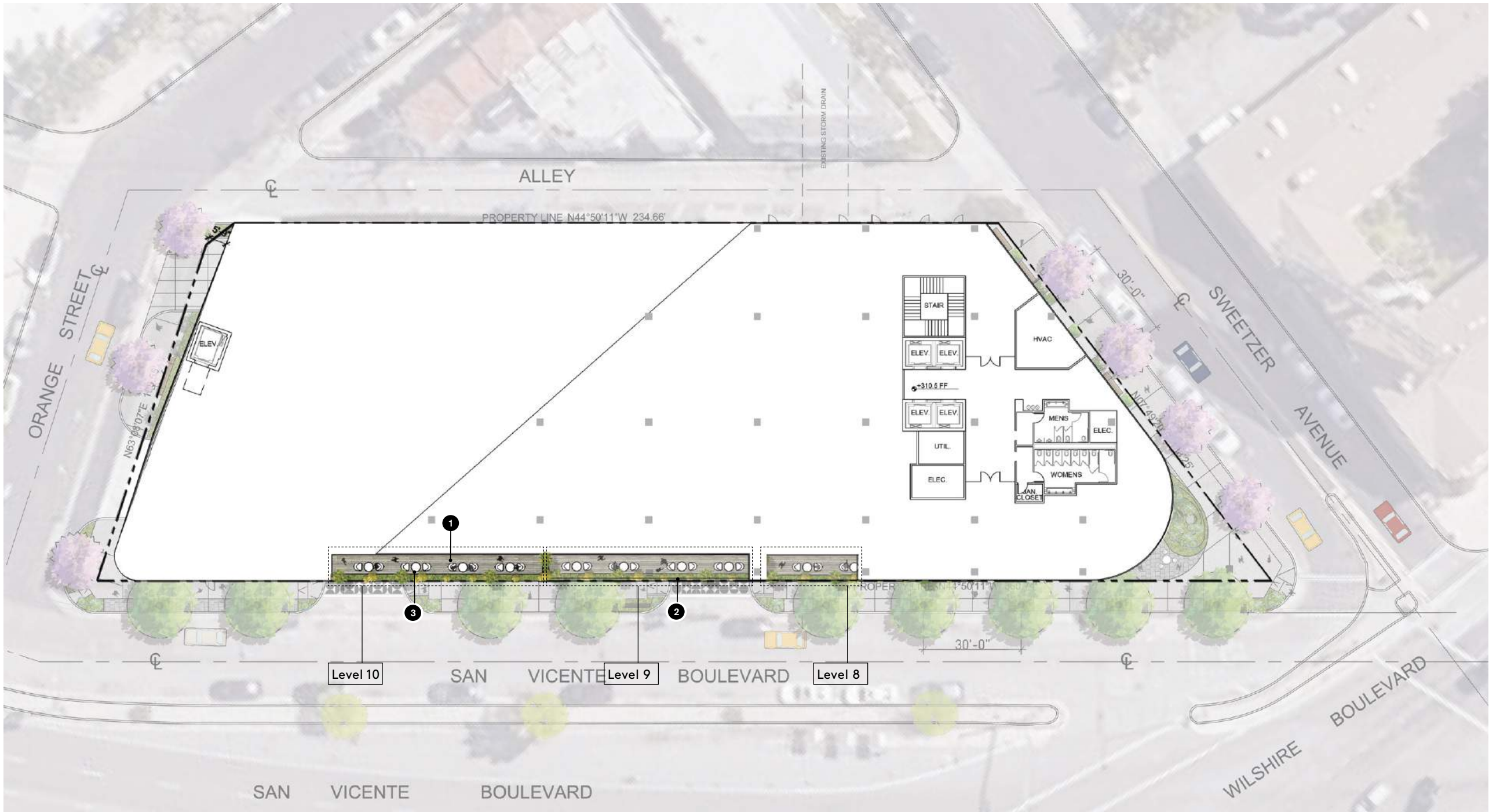
Golden Sedge
Size: 1 gallon Spaced at 18" O.C.



Trumpet Vine Trailing Variety
Size: 5 Gallon Spaced at 30" O.C.



Bougainvillea White Trailing Variety
Size: 5 Gallon Spaced at 30" O.C.



SCALE 1/32" = 1'-0"



KEYNOTES:

- 1. Wood decking over structure
- 2. Raised planters w/ drought-tolerant plants
- 3. Loose seating

Note: All planting areas to be irrigated with weather-sensing and automated water efficient drip irrigation system.

EXHIBIT "A"

Page No. 9 of 27

Case No. CPC-2017-467-GPA-VZC-HD-SPR

June 23, 2022



Oriental Fountain Grass
Size: 5 gallon Spaced at 24" O.C.



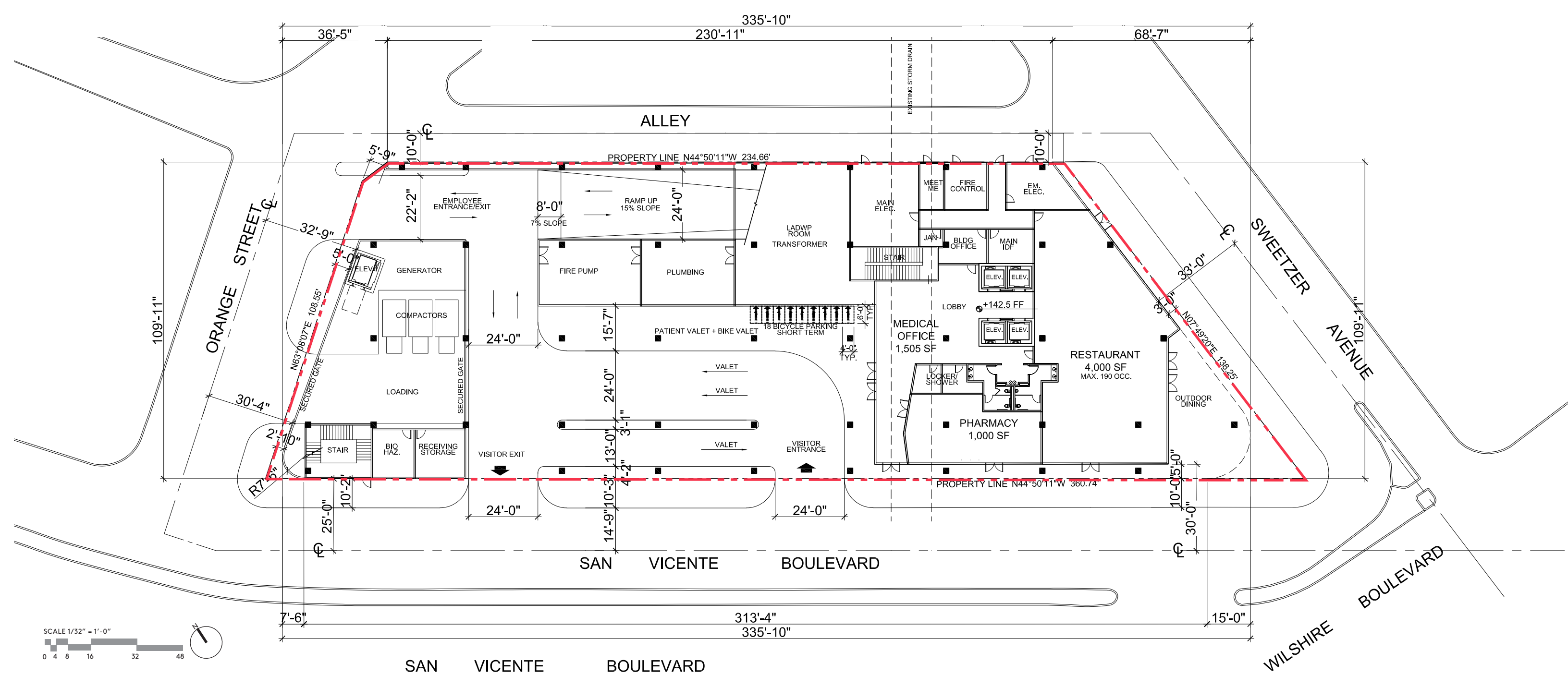
Golden Sedge
Size: 1 gallon Spaced at 18" O.C.



Trumpet Vine Trailing Variety
Size: 5 Gallon Spaced at 30" O.C.



Bougainvillea White Trailing Variety
Size: 5 Gallon Spaced at 30" O.C.



PROJECT INFORMATION

SITE ADDRESSES 650-676 SOUTH SAN VICENTE BOULEVARD LOS ANGELES, CALIFORNIA 90048	
656 SOUTH SAN VICENTE MEDICAL OFFICE PROJECT 145,305 SF MEDICAL OFFICE BUILDING GROUND FLOOR COMMERCIAL SPACE (RETAIL, RESTAURANT)	
ACCESSOR PARCEL NUMBERS 5510-022-033, 5510-022-034, 55100-22-035, 55100-22-058, 55100-22-059 ZONING AND GENERAL PLAN LAND USE DESIGNATION EXISTING C1-1VL-0 LIMITED COMMERCIAL PROPOSED (Q) C2-2D REGIONAL CENTER COMMERCIAL	
LOT AREA AND BUILDABLE AREA TOTAL EXISTING LOT AREA 33,060 SF 0.76 ACRE) BUILDABLE AREA (EXCLUDES DEDICATION AREA) 32,290 SF	
OPEN SPACE OPEN SPACE REQUIRED C1-1VL-0 (EXISTING ZONING) 0 SF	
OPEN SPACE PROPOSED (Q) C2-2D (PROPOSED ZONING)	
LEVEL 10	533 SF
LEVEL 9	533 SF
LEVEL 8	570 SF
LEVEL 7	328 SF
LEVEL 6	1,864 SF
GROUND LEVEL	815 SF
TOTAL OPEN SPACE	4,643 SF

LEGAL DESCRIPTION

(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-797214-LA2 DATED JUNE 3, 2016)	
THE LAND REFERRED TO IN THIS COMMITMENT IS SITUATED THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:	
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APN: 5510-022-058 (AFFECTS: LOTS 5 AND 6) AND 5510-022-059 (AFFECTS: LOTS 3 AND 4)	
AND	
(PER FIRST AMERICAN TITLE COMPANY COMMITMENT NO. NCS-811256-LA2 DATED OCTOBER 13, 2016)	
LOTS 7, 8 AND 9 OF BLOCK 4, TRACT NO. 7555, IN THE CITY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 80 PAGES 51 TO 53 INCLUSIVE OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.	
APN: 5510-022-023 (AFFECTS LOT 7) 5510-022-034 (AFFECTS LOT 8) 5510-022-035 (AFFECTS LOTS 9)	
TREES	
TREES REQUIRED C1-1VL-0 (EXISTING ZONING)	0 TREES
TREES PROPOSED (Q) C2-2D (PROPOSED ZONING)	16 TREES

FLOOR AREA

ALLOWABLE BASE FLOOR AREA C1-1VL-0 (EXISTING ZONING) FAR 1.5:1 (BUILDABLE AREA 32,290 SF X 1.5)						48,435 SF
PROPOSED FLOOR AREA (Q) C2-2D (PROPOSED ZONING) FAR 4.5:1 (BUILDABLE AREA 32,290 SF X 4.5)						145,305 SF
LEVEL	MEDICAL OFFICE	RETAIL	RESTAURANT	UTILITY (excluded)	PARKING (excluded)	
LEVEL 12	6,200 SF	--	--	2,085 SF	--	
LEVEL 11	9,600 SF	--	--	2,085 SF	--	
LEVEL 10	13,000 SF	--	--	2,085 SF	--	
LEVEL 9	27,000 SF	--	--	2,085 SF	--	
LEVEL 8	27,000 SF	--	--	2,085 SF	--	
LEVEL 7	28,000 SF	--	--	2,085 SF	--	
LEVEL 6	28,000 SF	--	--	2,085 SF	--	
LEVEL 5	--	--	--	--	32,410 SF	
LEVEL 4	--	--	--	--	32,410 SF	
LEVEL 3	--	--	--	--	32,410 SF	
LEVEL 2	--	--	--	--	32,410 SF	
GROUND LEVEL	1,505 SF	1,000 SF	4,000 SF	11,650 SF	-- SF	
SUBTOTAL	140,305 SF	1,000 SF	4,000 SF	26,245 SF	129,640 SF	
FLOOR AREA SUMMARY (RETAIL, RESTAURANT + MEDICAL OFFICE)						145,305 SF

HEIGHT

HEIGHT LIMIT C1-1VL-0 (EXISTING ZONING)	45 FT
HEIGHT PROPOSED (Q) C2-2D (PROPOSED ZONING)	230 FT

VEHICLE PARKING

PARKING REQUIRED					
USE	AREA	PARKING RATIO REQUIRED (stalls per 1,000 SF)	STALLS REQUIRED	STALLS AFTER 20% REDUCTION*	STALLS AFTER FURTHER 30% REDUCTION**
MEDICAL OFFICE	140,305 SF	5	702	561	393
RETAIL	1,000 SF	4	4	3	2
RESTAURANT	4,000 SF	10	40	32	22
	145,305	5.1	746	596	417

* City's Discretionary Parking Reduction for Commercial Projects
** Transit Priority Area Reduction (within 1,500 ft of a transit station)

417 TOTAL PARKING STALLS REQUIRED
418 TOTAL PARKING STALLS PROVIDED

BICYCLE PARKING

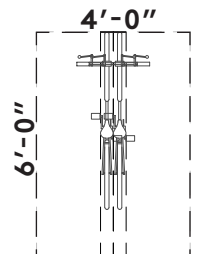
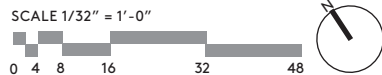
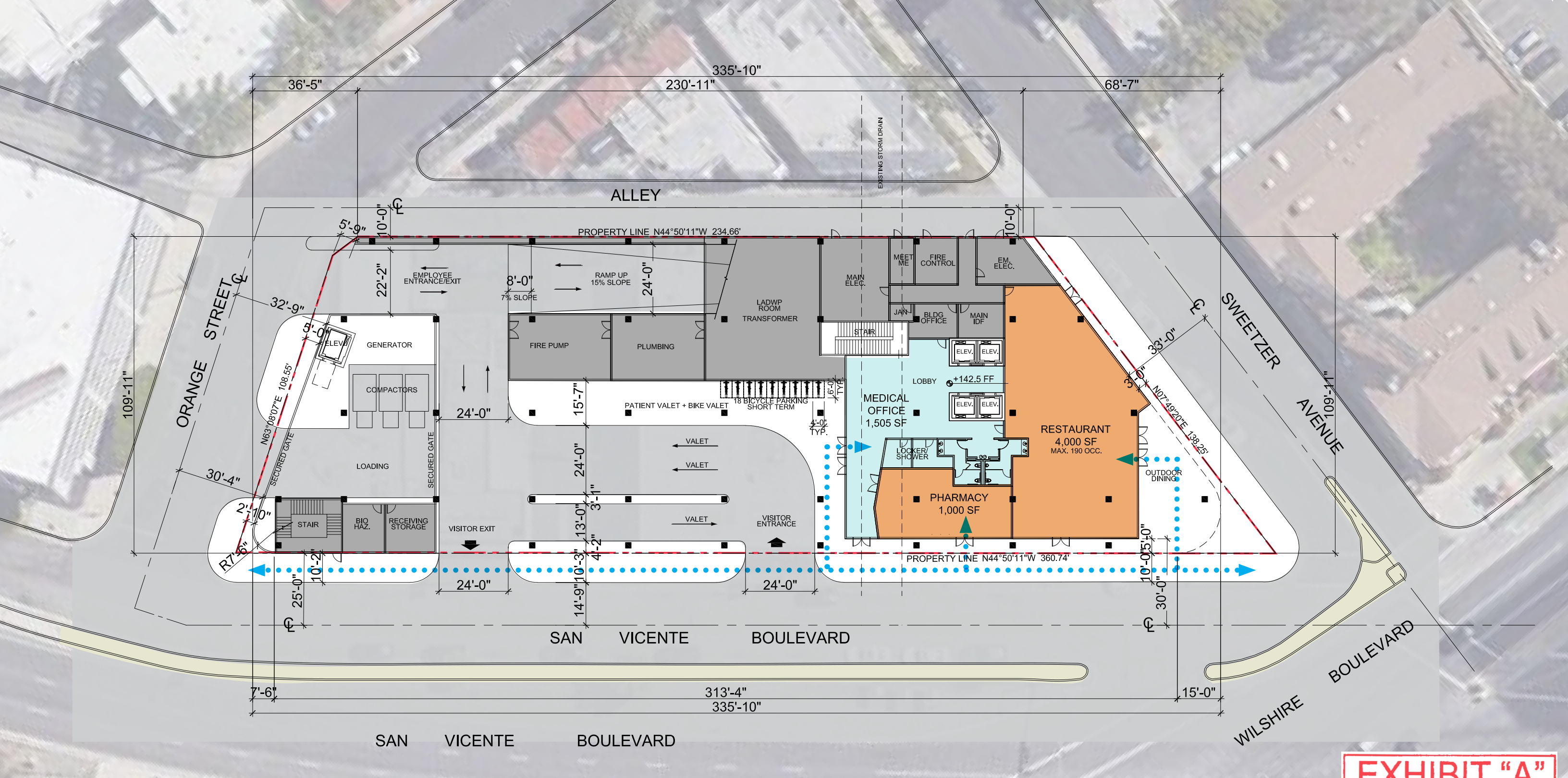
BICYCLE PARKING REQUIRED	716 BICYCLE PARKING SPACES
BICYCLE PARKING PROVIDED	716 BICYCLE PARKING SPACES

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BICYCLE RACK = 2 BICYCLES

--- PROPERTY LINE
←.....→ PATH OF TRAVEL

- UTILITY
- RETAIL + RESTAURANT
- MEDICAL OFFICE

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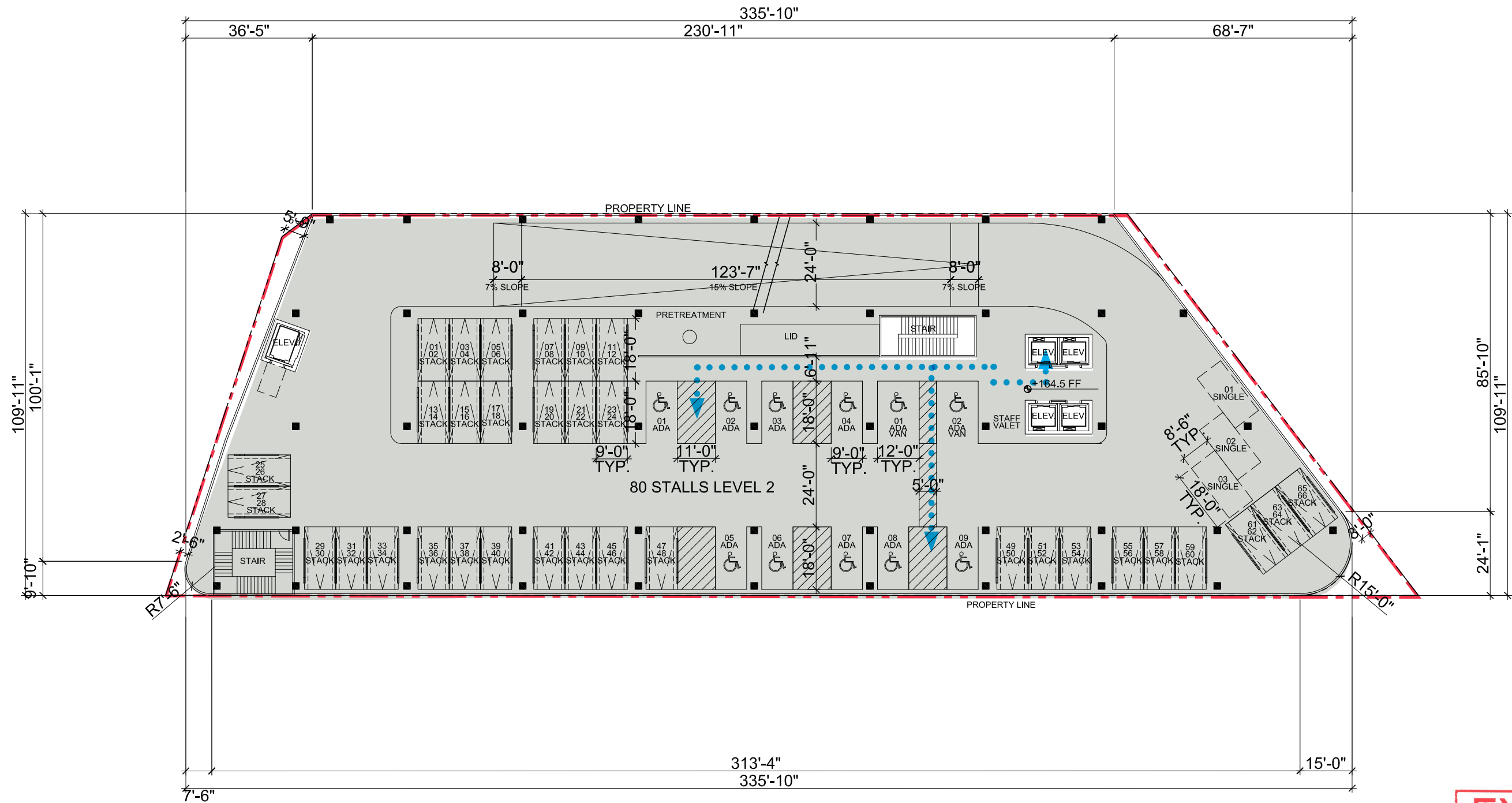


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June 23, 2022

--- PROPERTY LINE
--- PATH OF TRAVEL
PARKING

SCALE 1/32" = 1'-0"
0 4 8 16 32 48

rdc. | studioneleven | ZGF

CLIENT
650 SSV Property Owner, LLC
656-676 SSV Property Owner, LLC

PROJECT
656 South San Vicente Medical Office Project
650 - 676 South San Vicente Blvd, Los Angeles, CA 90048

07.01.2020
LEVEL 2
Entitlement Drawings

A-9
19.249

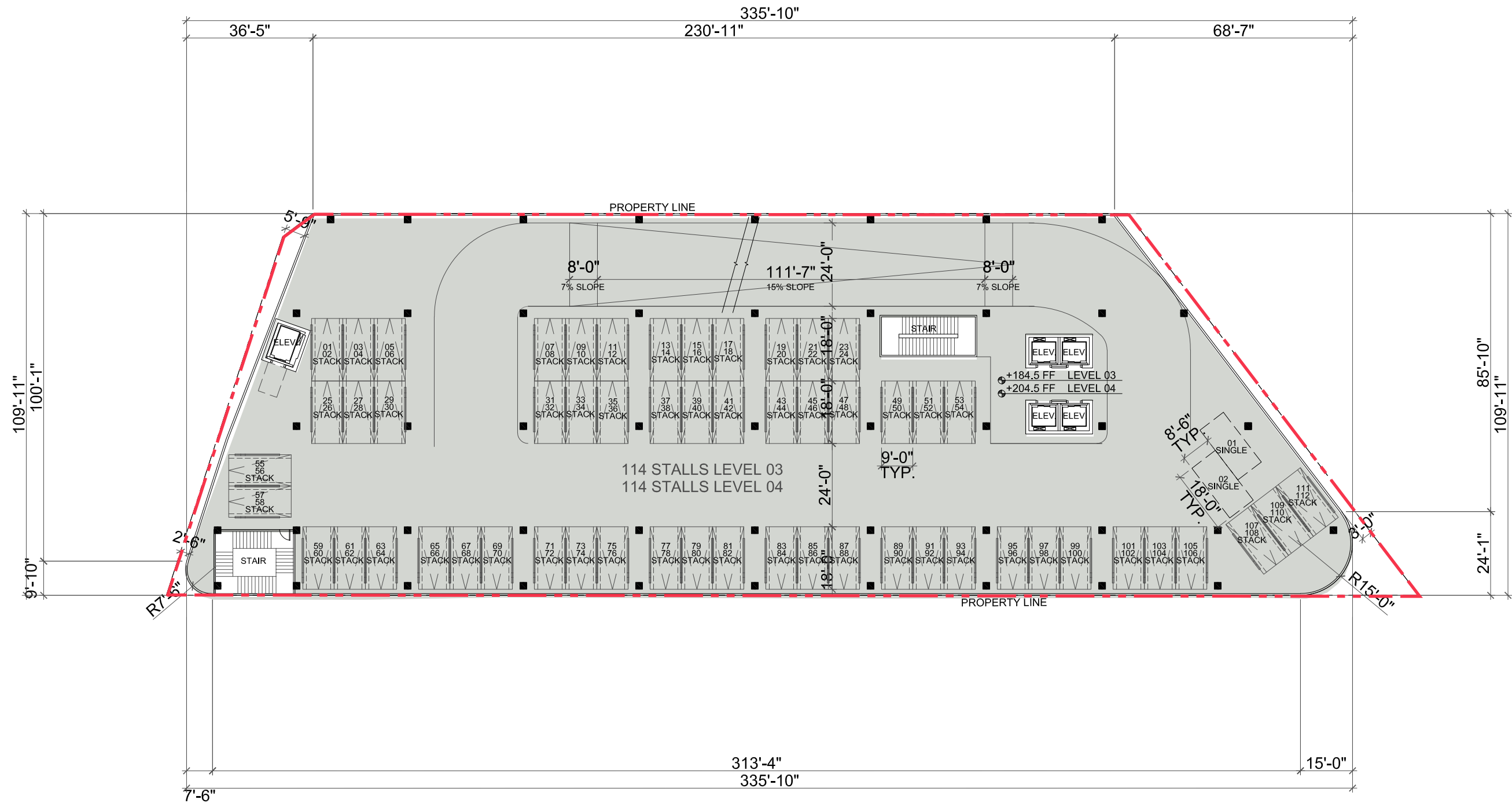
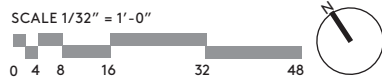


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--- PROPERTY LINE

PARKING

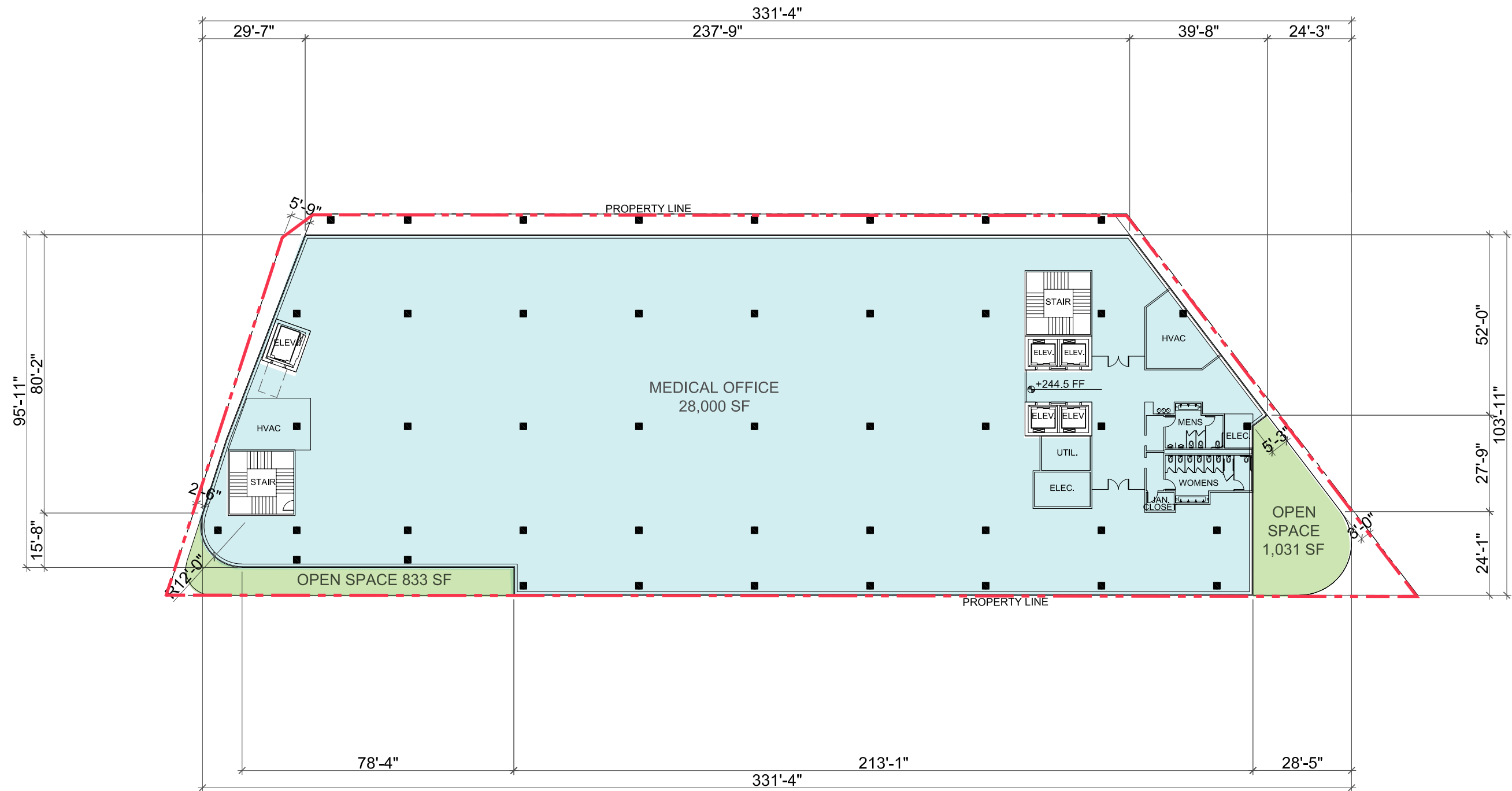
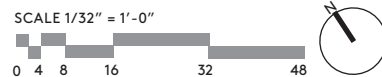


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- - - - - PROPERTY LINE
- OPEN SPACE
- MEDICAL OFFICE

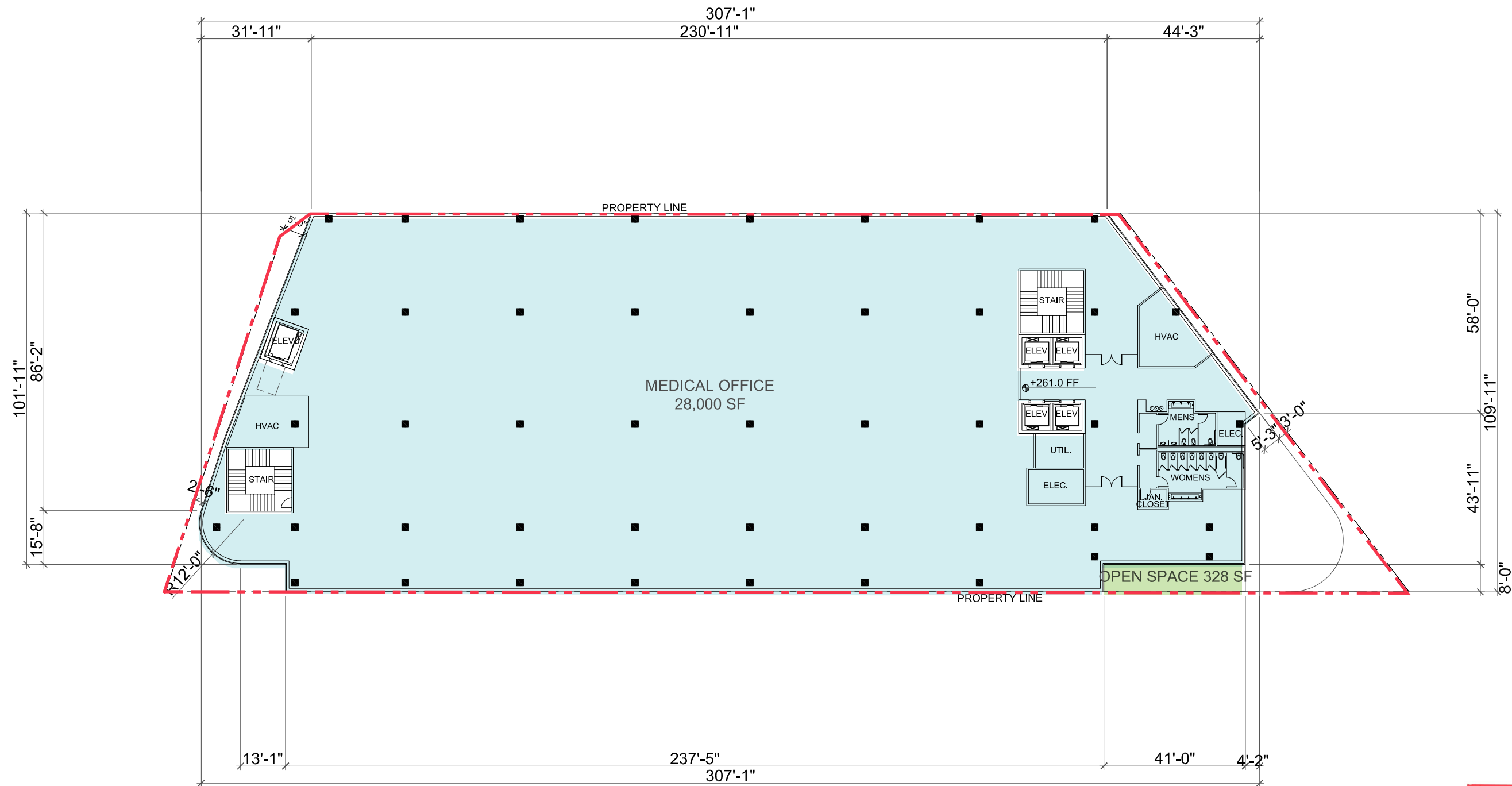
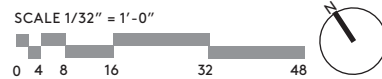


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June 23, 2022



- PROPERTY LINE
- OPEN SPACE
- MEDICAL OFFICE

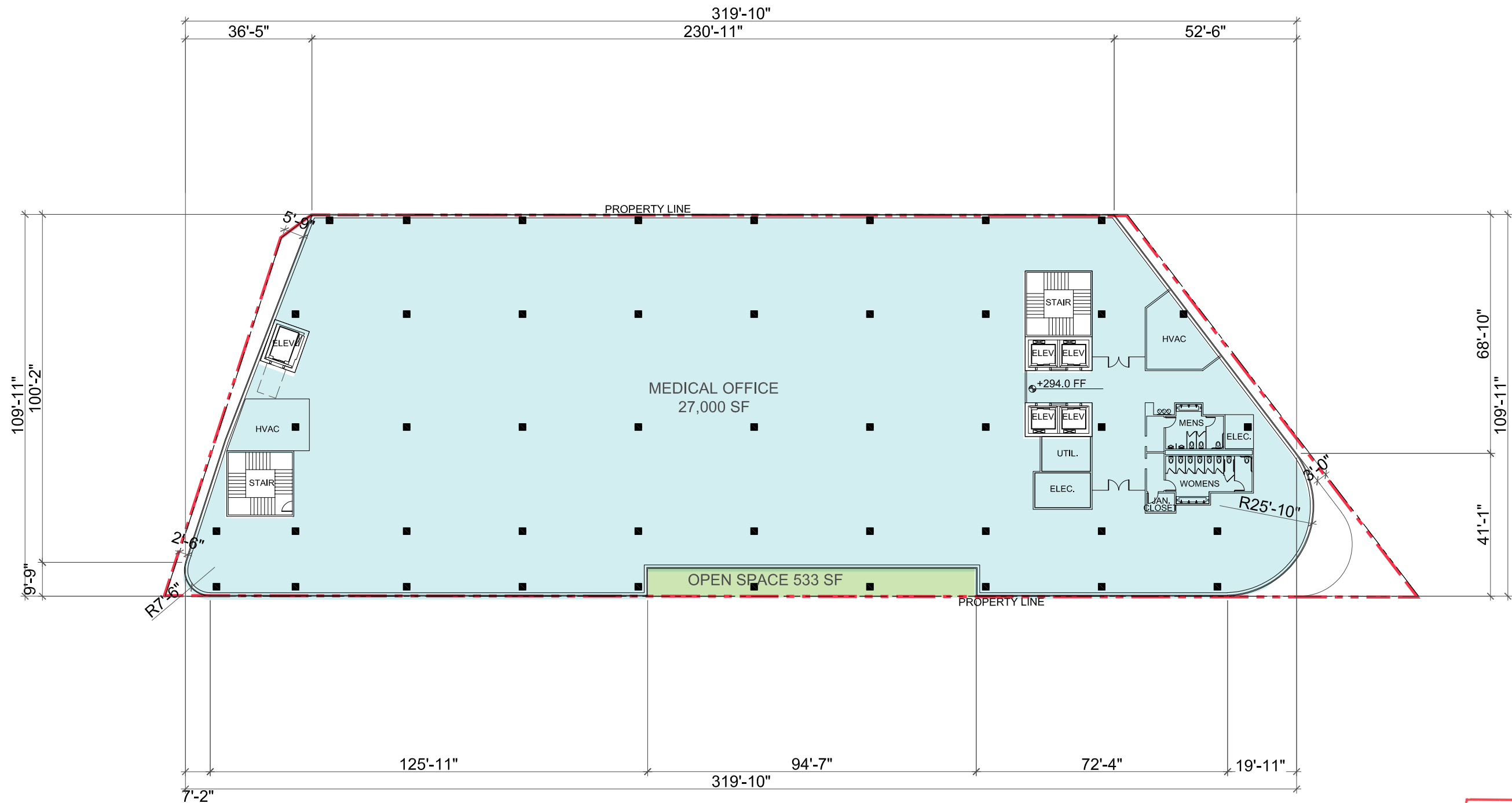
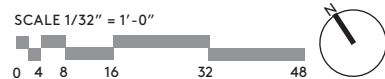


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- PROPERTY LINE
- OPEN SPACE
- MEDICAL OFFICE

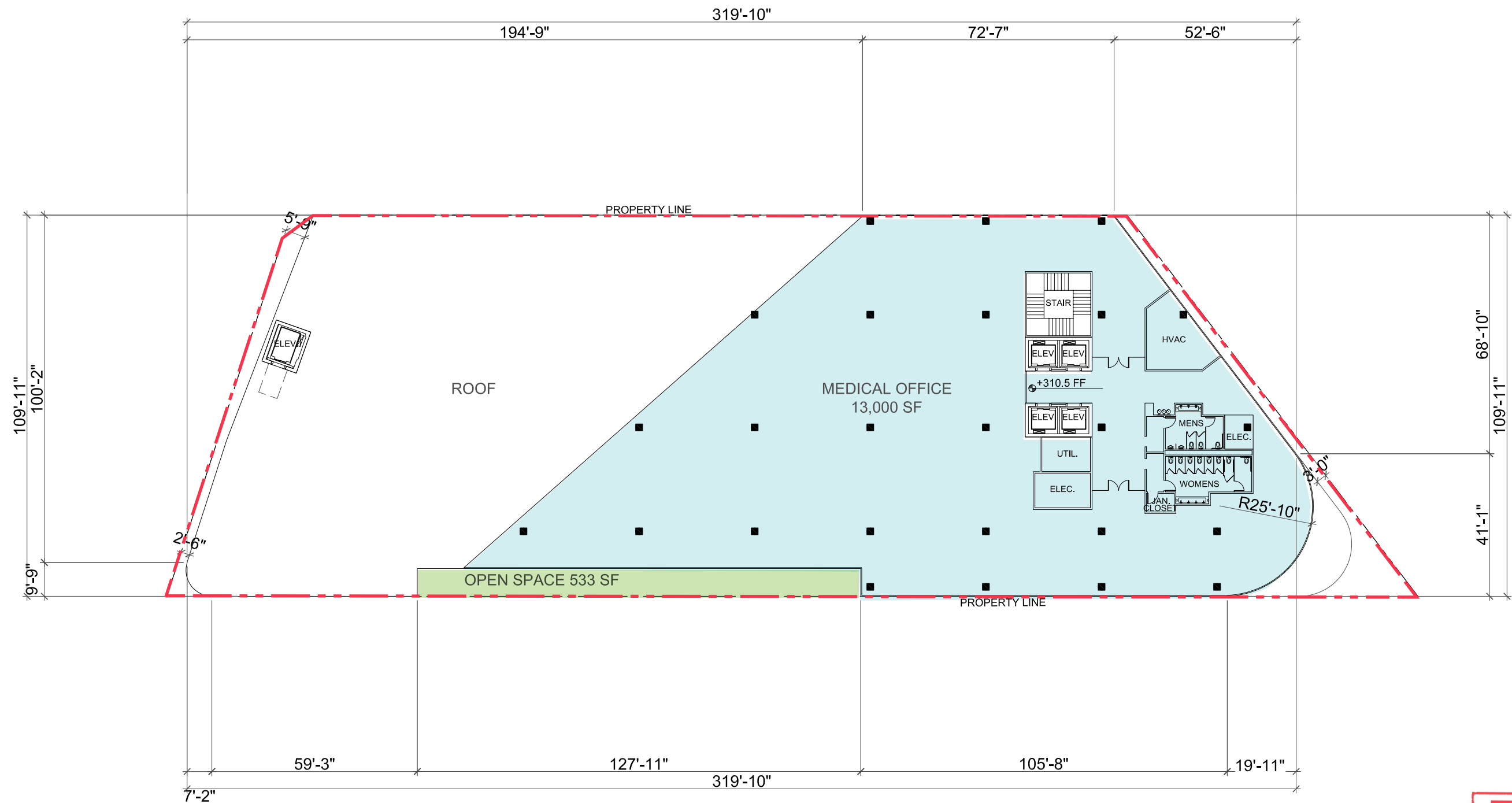
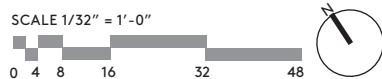


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- PROPERTY LINE
- OPEN SPACE
- MEDICAL OFFICE

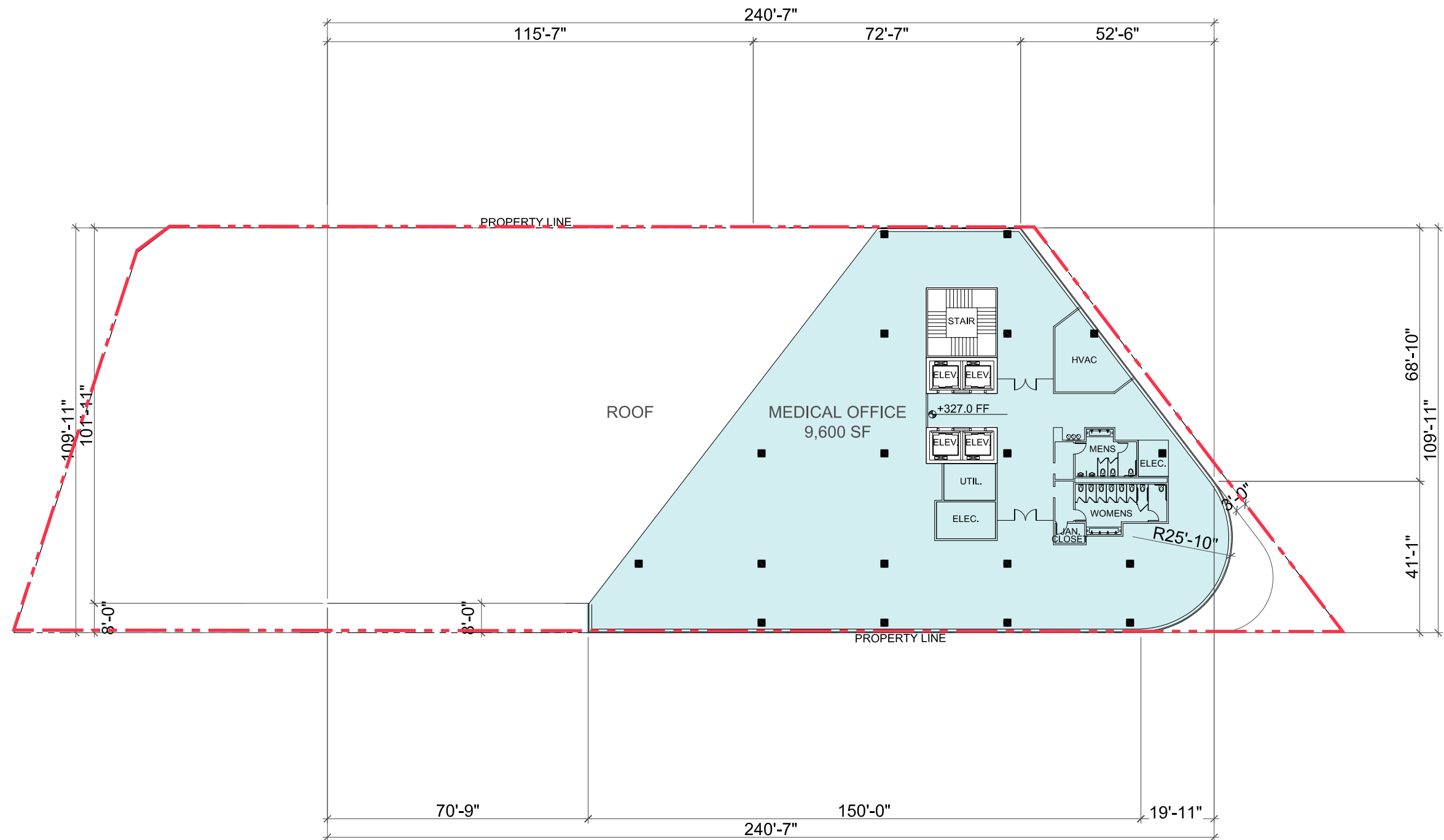
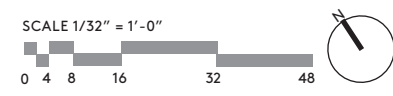
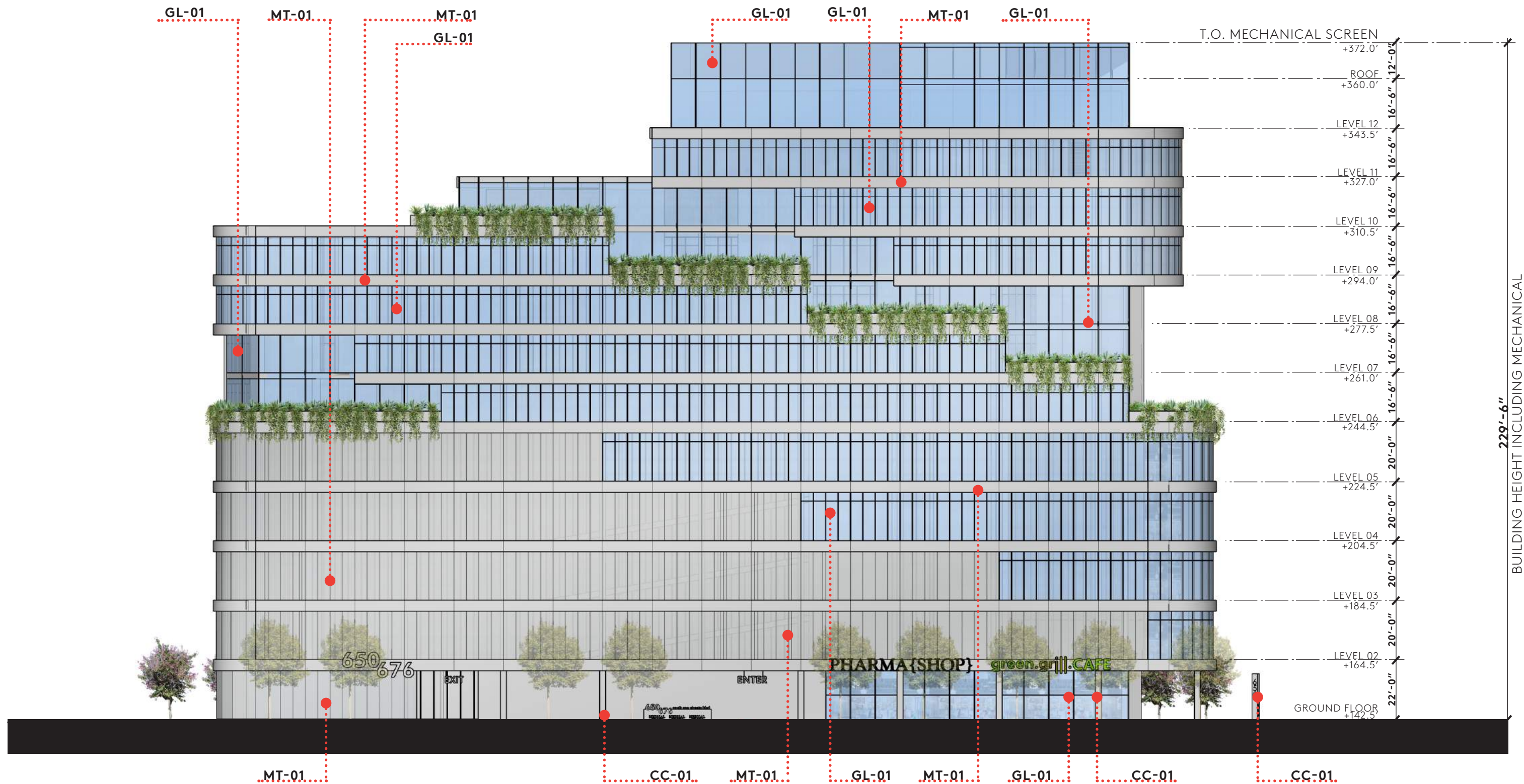


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June 23, 2022

--- PROPERTY LINE
 MEDICAL OFFICE





SCALE 1/32" = 1'-0"

0 4 8 16 32 48

EXHIBIT "A"

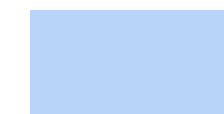
Page No. 23 of 27

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June 23, 2022

MATERIAL LEGEND

COLORS IN THIS EXHIBIT ARE ILLUSTRATIVE AND FOR GRAPHIC INTENT ONLY.



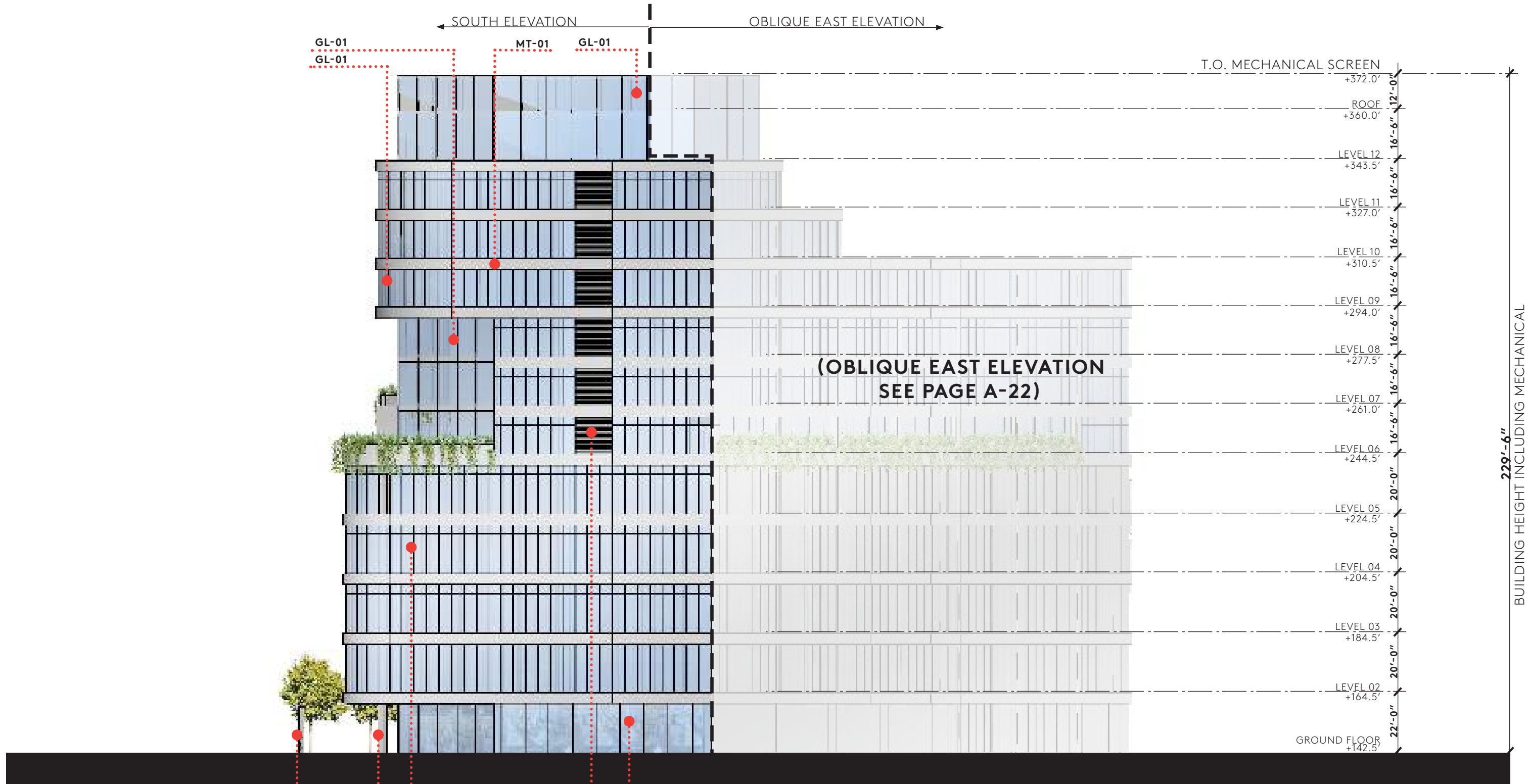
GL-01
GLAZING SYSTEM



MT-01
METAL PANEL



CC-01
EXPOSED CONCRETE
COLUMNS



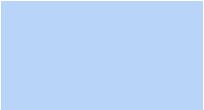


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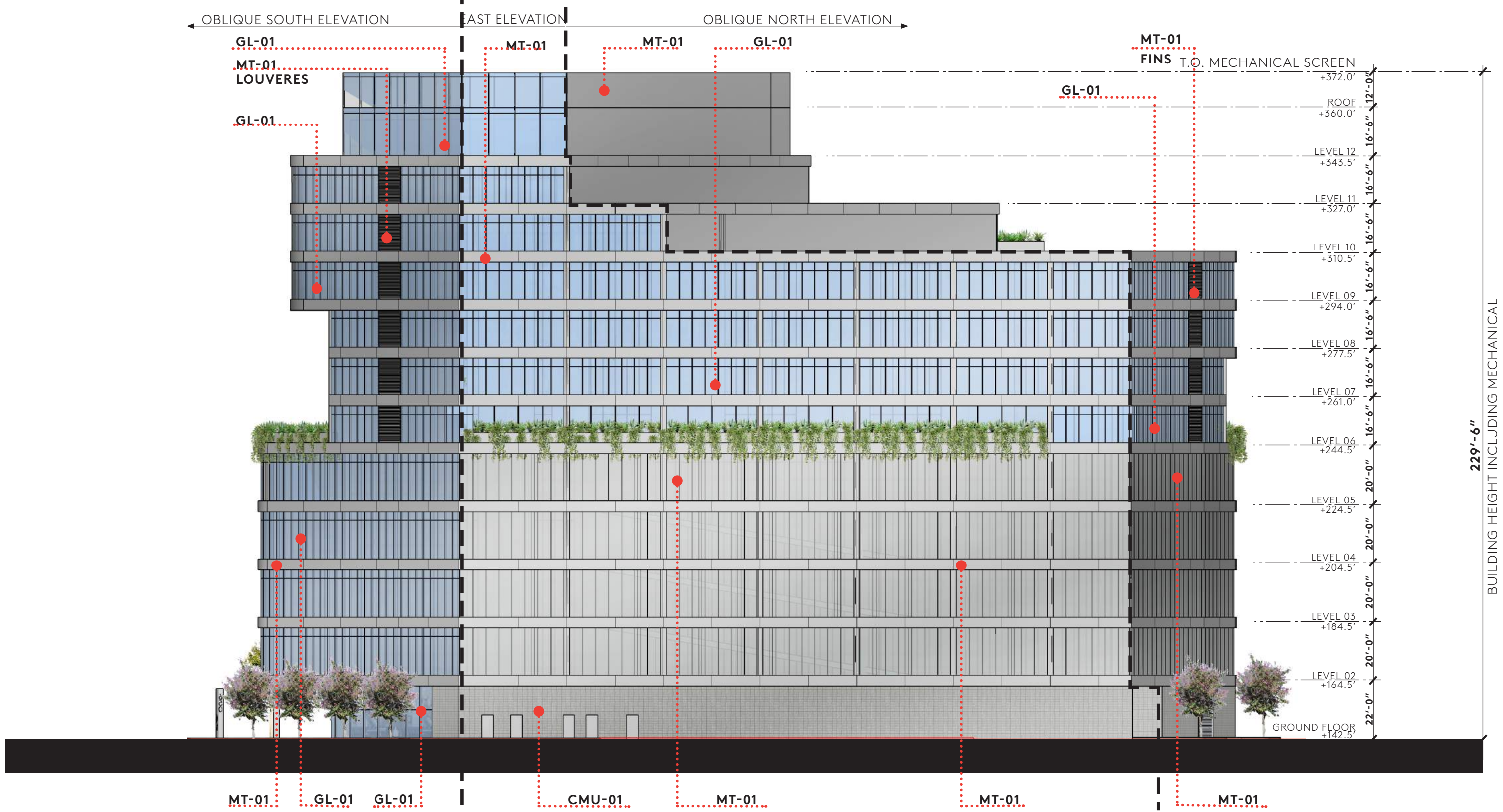
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MATERIAL LEGEND
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GL-01 GLAZING SYSTEM	MT-01 METAL PANEL	CC-01 EXPOSED CONCRETE COLUMNS

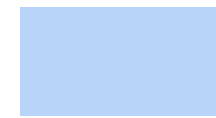


SCALE 1/32" = 1'-0"

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June 23, 2022

MATERIAL LEGEND
COLORS IN THIS EXHIBIT ARE ILLUSTRATIVE AND FOR GRAPHIC INTENT ONLY.



GL-01
GLAZING SYSTEM



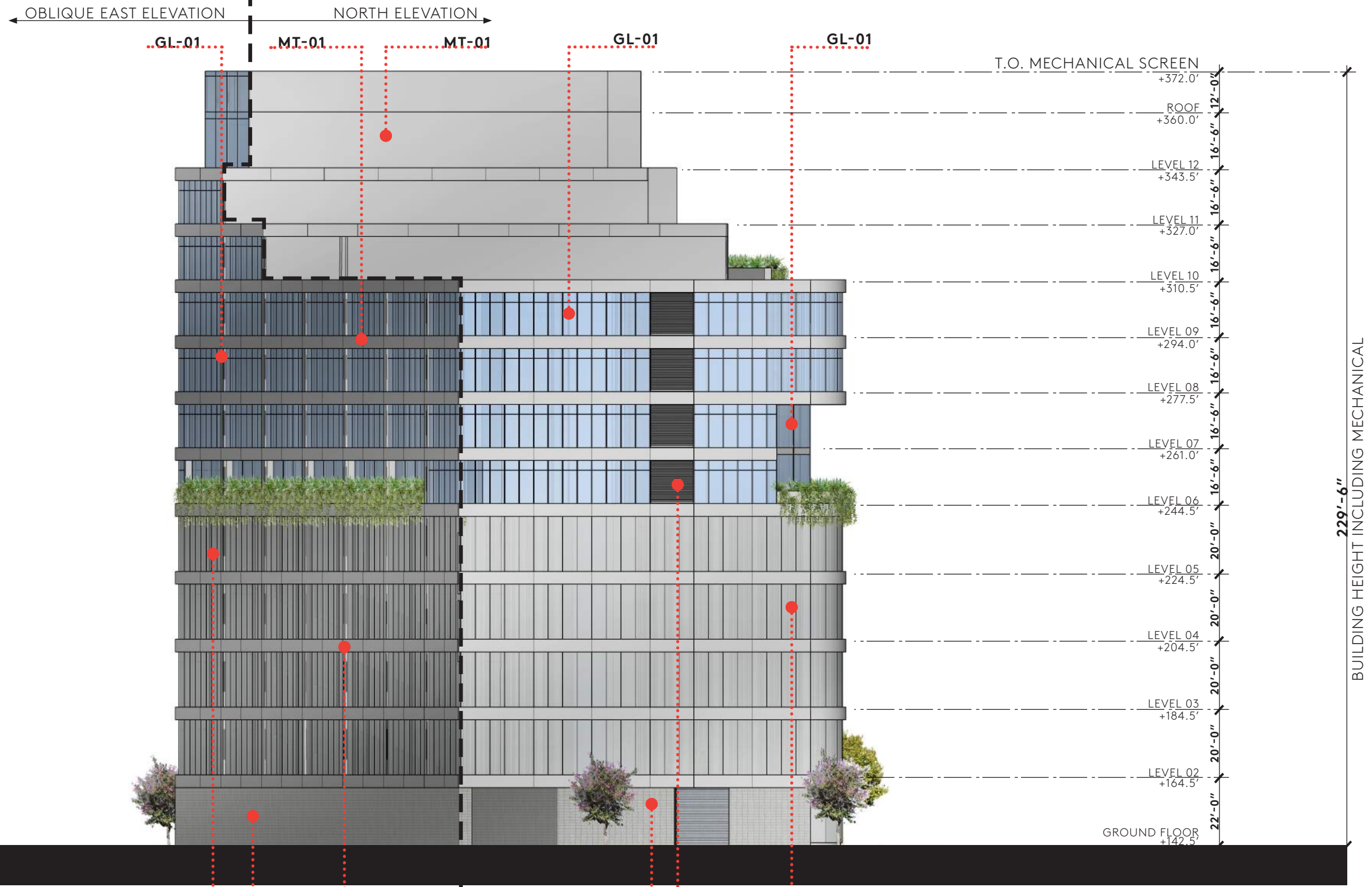
MT-01
METAL PANEL



CC-01
EXPOSED CONCRETE
COLUMNS



CMU-01
CMU WALL



SCALE 1/16" = 1'-0"

SCALE 1/32" = 1'-0"

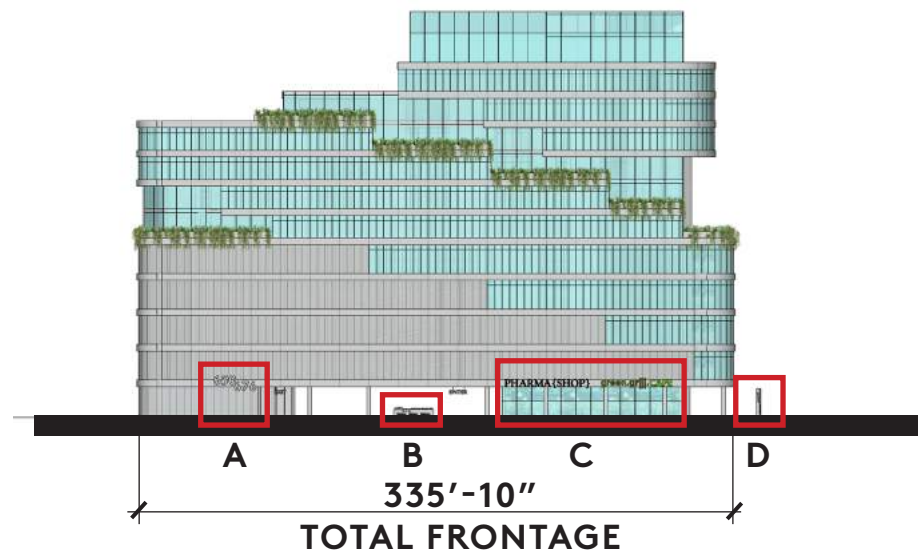
EXHIBIT "A"
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June 23, 2022

LOUVERS

MATERIAL LEGEND
COLORS IN THIS EXHIBIT ARE ILLUSTRATIVE AND FOR GRAPHIC INTENT ONLY.

			
GL-01 GLAZING SYSTEM	MT-01 METAL PANEL	CC-01 EXPOSED CONCRETE COLUMNS	CMU-01 CMU WALL



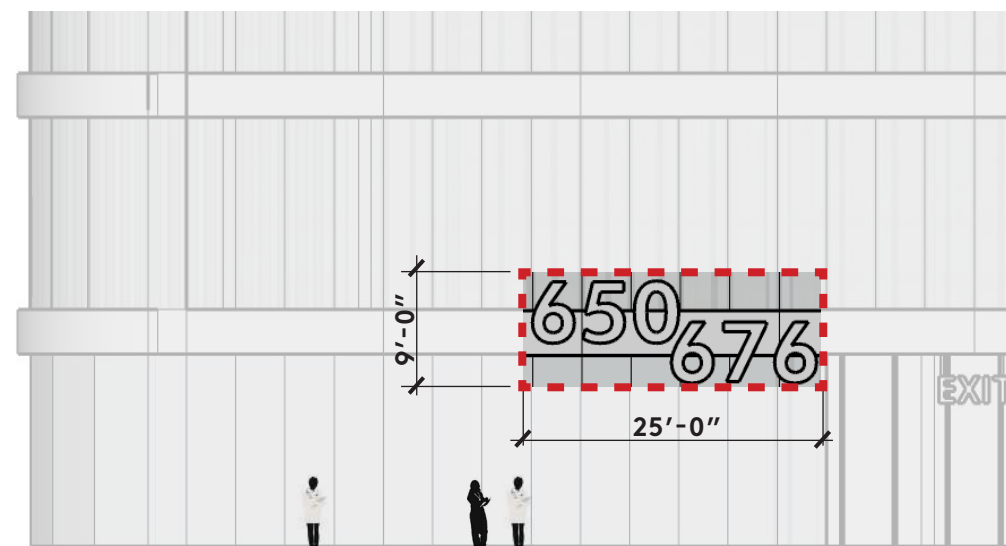
PROJECT NOT TO EXCEED ALLOWABLE SIGN HEIGHT, AREA, LOCATION OR STRUCTURE PER LOS ANGELES DEPARTMENT OF BUILDING SAFETY

L.A.M.C. SECTION 14.4.2 & 14.4.10; L.A.B.C SECTION 6211

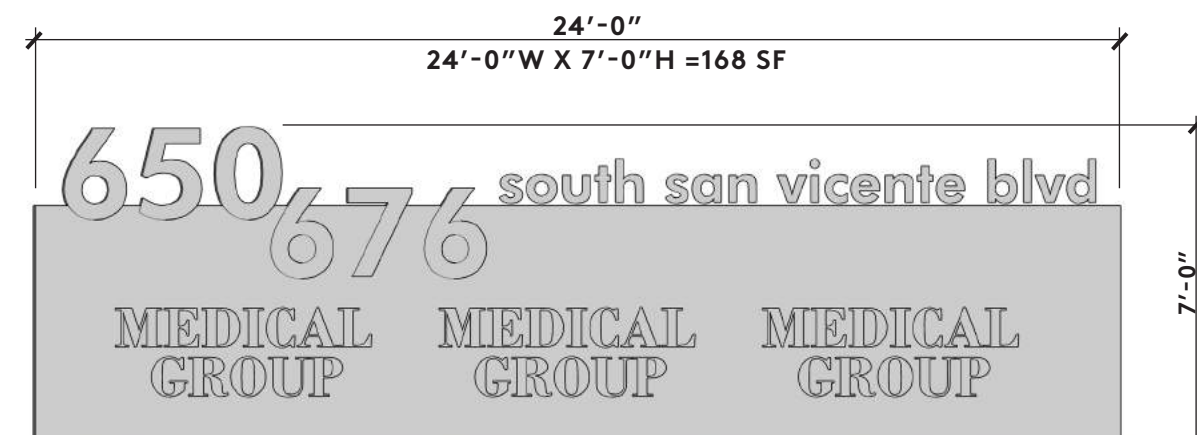
COMBINED AREA OF WALL, PROJECTING, MONUMENT, ILLUMINATED ARCHITECTURAL CANOPY, POLE, ROOF, AND WINDOW SIGNS SHALL NOT EXCEED 4 SQUARE FOOR PER FOOT OF STREET FRONTAGE.

335'-10" TOTAL FRONTAGE X 4 SF = **1,343 SF TOTAL SIGNAGE AREA ALLOWED**

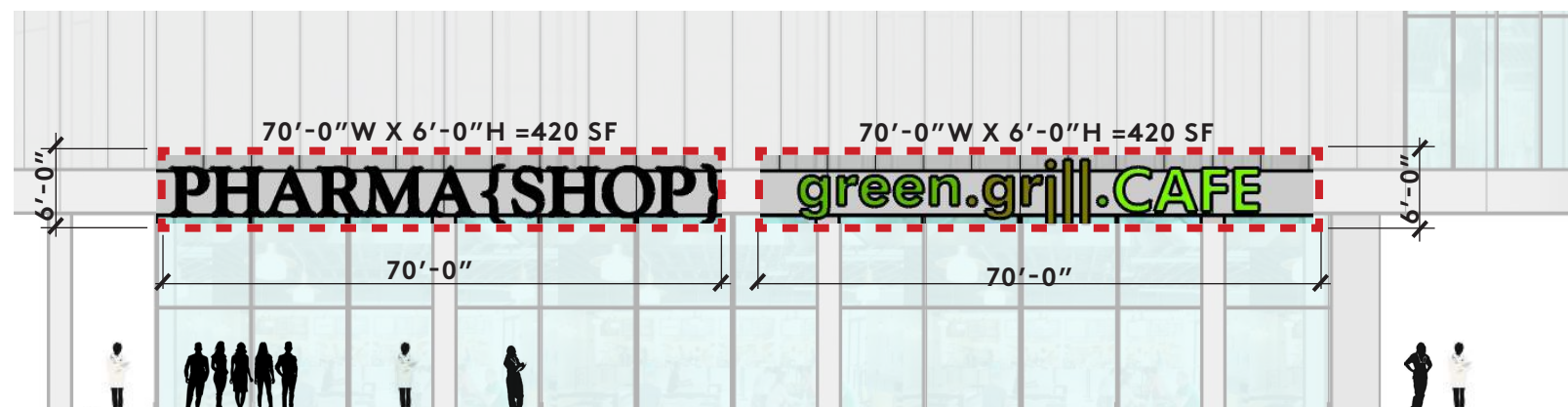
A. WALL SIGN	225 SF	
B. MONUMENT SIGN	168 SF	
C. WALL SIGN	840 SF	
D. POLE SIGN	60 SF	
TOTAL SIGNAGE AREA	1,293 SF	TOTAL SIGNAGE AREA PROPOSED



A. WALL SIGNAGE: 1/16"= 1'-0" 225 SF



B. MONUMENT SIGN: 1/4"= 1'-0" 168 SF



C. WALL SIGN: 1/16"= 1'-0" 840 SF

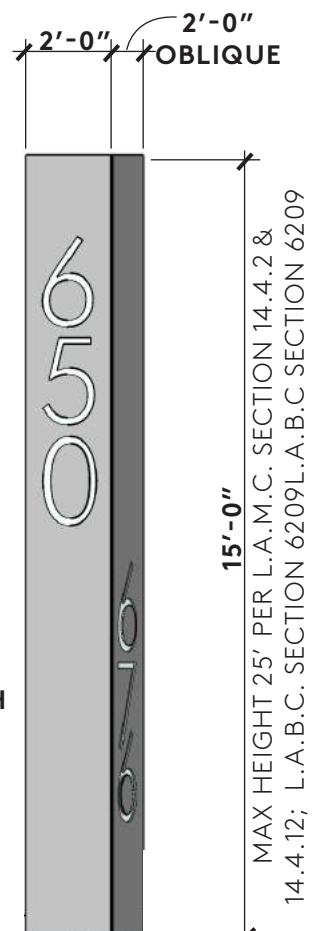


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2'-0" W X 15'-0" H
=30 SF
X2 SIDES = 60 SF

D. POLE SIGN: 1/4"= 1'-0" 60 SF







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MITIGATION MONITORING PROGRAM

1. Introduction

This Mitigation Monitoring Program (MMP) has been prepared pursuant to Public Resources Code (PRC) Section 21081.6, which requires a Lead Agency to adopt a “reporting or monitoring program for changes to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment.” In addition, California Environmental Quality Act (CEQA) Guidelines Section 15097(a) requires that a public agency adopt a program for monitoring or reporting mitigation measures and project revisions, which it has required to mitigate or avoid significant environmental effects. This MMP has been prepared in compliance with the requirements of CEQA, PRC Section 21081.6 and CEQA Guidelines Section 15097.

The City of Los Angeles is the Lead Agency for the Project and, therefore, is responsible for administering and implementing the MMP. A public agency may delegate reporting or monitoring responsibilities to another public agency or to a private entity that accepts the delegation; however, until mitigation measures have been completed, the Lead Agency remains responsible for ensuring that implementation of the mitigation measures occurs in accordance with the program.

An Environmental Impact Report (EIR) has been prepared to address the potential environmental impacts of the Project. The evaluation of the Project’s impacts takes into consideration the project design features (PDF) and identifies mitigation measures (MM) needed to avoid or reduce potentially significant environmental impacts. This MMP is designed to monitor implementation of the PDFs and MMs identified for the Project.

2. Purpose

The intent of this MMP is to:

1. Verify compliance with the project design features and mitigation measures identified in the EIR;
2. Provide a framework to document implementation of identified project design features and mitigation measures;
3. Provide a record of mitigation requirements;
4. Identify monitoring and enforcement agencies;
5. Establish and clarify administrative procedures for the clearance of project design features and mitigation measures;
6. Establish the frequency and duration of monitoring; and
7. Utilize the existing agency review processes wherever feasible.

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3. Organization

As shown on the following pages, each identified project design feature and mitigation measure for the Project is listed and categorized by environmental impact area, with accompanying identification of the following:

- **Enforcement Agency:** the agency with the power to enforce the PDF or MM.
- **Monitoring Agency:** the agency to which reports involving feasibility, compliance, implementation, and development are made.
- **Monitoring Phase:** the phase of the Project during which the PDF or MM shall be monitored.
- **Monitoring Frequency:** the frequency at which the PDF or MM shall be monitored.
- **Action Indicating Compliance:** the action by which the Enforcement or Monitoring Agency indicates that compliance with the identified PDF or required MM has been implemented.

4. Administrative Procedures and Enforcement

This MMP shall be enforced throughout all phases of the Project. The Applicant shall be responsible for implementing each PDF and MM and shall be obligated to provide certification, as identified below, to the appropriate monitoring and enforcement agencies that each PDF and MM has been implemented. The Applicant shall maintain records demonstrating compliance with each PDF and MM. Such records shall be made available to the City upon request.

During the construction phase and prior to the issuance of building permits, the Applicant shall retain an independent Construction Monitor (either via the City or through a third-party consultant), approved by the Department of City Planning, who shall be responsible for monitoring implementation of PDFs and MMs during construction activities consistent with the monitoring phase and frequency set forth in this MMP.

The Construction Monitor shall also prepare documentation of the Applicant's compliance with the PDFs and MMs during construction every 90 days in a form satisfactory to the Department of City Planning. The documentation must be signed by the Applicant and Construction Monitor and be included as part of the Applicant's Compliance Report. The Construction Monitor shall be obligated to immediately report to the Enforcement Agency any non-compliance with the MMs and PDFs within two businesses days if the Applicant does not correct the non-compliance within a reasonable time of notification to the Applicant by the monitor or if the non-compliance is repeated. Such non-compliance shall be appropriately addressed by the Enforcement Agency.

5. Program Modification

After review and approval of the final MMP by the Lead Agency, minor changes and modifications to the MMP are permitted, but can only be made subject to City approval.

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The Lead Agency, in conjunction with any appropriate agencies or departments, will determine the adequacy of any proposed change or modification. This flexibility is necessary in light of the nature of the MMP and the need to protect the environment. No changes will be permitted unless the MMP continues to satisfy the requirements of CEQA, as determined by the Lead Agency.

The Project shall be in substantial conformance with the PDFs and MMs contained in this MMP. The enforcing departments or agencies may determine substantial conformance with PDFs and MMs in the MMP in their reasonable discretion. If the department or agency cannot find substantial conformance, a PDF or MM may be modified or deleted as follows: the enforcing department or agency, or the decision maker for a subsequent discretionary Project-related approval finds that the modification or deletion complies with CEQA, including CEQA Guidelines Sections 15162 and 15164, which could include the preparation of an addendum or subsequent environmental clearance, if necessary, to analyze the impacts from the modifications to or deletion of the PDFs or MMs. Any addendum or subsequent CEQA clearance shall explain why the PDF or MM is no longer needed, not feasible, or the other basis for modifying or deleting the PDF or MM, and that the modification will not result in a new significant impact consistent with the requirements of CEQA. Under this process, the modification or deletion of a PDF or MM shall not, in and of itself, require a modification to any Project discretionary approval unless the Director of Planning also finds that the change to the PDF or MM results in a substantial change to the Project or the non-environmental conditions of approval.

6. Mitigation Monitoring Program

a) Air Quality

(1) Project Design Features

No project design features are identified in the EIR for this environmental issue.

(2) Mitigation Measures

AIR-MM-1: The Applicant will implement the following construction equipment features for equipment operating at the Project Site. These features will be included in applicable bid documents, and successful contractor(s) must demonstrate the ability to supply such equipment. Construction features will include the following:

- For off-road diesel-powered construction equipment rated greater than 50 horse power: the equipment shall meet or exceed the CARB and USEPA Tier 4 off-road emissions standards or greater during Project construction or shall be fitted with an emissions control device that achieves diesel emissions reductions that are no less than what could be achieved by an EPA Tier 4 Final engine.
- The Project Applicant shall implement the use of alternatively fueled equipment to the extent feasible for equipment greater than 50 horsepower. Equipment less than 50 horsepower shall be electric plug-in, solar-powered, or alternative fueled (i.e.,

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non-diesel). Pole power shall be made available for use of electric tools, equipment, lighting, etc. These requirements shall be included in applicable bid documents and successful contractor(s) must demonstrate the ability to supply such equipment.

- Alternative-fueled generators will be used when commercial models that have the power supply requirements to meet the construction needs of the Project are commercially available from local suppliers/vendors, and on-site electrical power is not available. The determination of the commercial availability of such equipment will be made by the City prior to the issuance of grading or building permits based on Applicant-provided evidence of the availability or unavailability of alternative-fueled generators and/or evidence obtained by the City from expert sources such as construction contractors in the region.
 - A copy of each unit's certified tier specification or model year specification and CARB or SCAQMD operating permit (if applicable) shall be available upon request at the time of mobilization of each applicable unit of equipment. Equipment shall be properly serviced and maintained in accordance with the manufacturer's recommendations. Construction contractors shall also ensure that all nonessential idling of construction equipment is restricted to five minutes or less in compliance with California Air Resources Board's Rule 2449.
- **Enforcement Agency:** City of Los Angeles Department of Building and Safety
 - **Monitoring Agency:** City of Los Angeles Department of Building and Safety; City of Los Angeles Department of City Planning
 - **Monitoring Phase:** Pre-construction; Construction
 - **Monitoring Frequency:** Once during Project plan check; Continuous field inspections during construction, with quarterly reporting
 - **Action Indicating Compliance:** Issuance of applicable building permit; Field inspection sign-off

b) Cultural Resources

(1) Project Design Features

No project design features are identified in the EIR for this environmental issue.

(2) Mitigation Measures

CUL-MM-1: Prior to the issuance of a demolition permit, the Applicant shall retain a qualified archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards to oversee an archaeological monitor who shall be present during construction excavations such as demolition, clearing/grubbing, grading, trenching, or any other construction excavation activity associated with the Project. The frequency of monitoring shall be based on the rate of excavation and grading activities, the materials being excavated (younger sediments vs. older sediments), the depth of excavation, and, if found, the abundance and type of archaeological resources

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encountered. Monitoring may be reduced to part-time inspections, or ceased entirely, if determined adequate by the qualified Archaeologist. At a minimum, the need for monitoring will be reassessed at depths of excavation greater than five feet below surface. Prior to commencement of excavation activities, an Archaeological Sensitivity Training shall be given for construction personnel. The training session, to be carried out by the qualified Archaeologist, will focus on how to identify archaeological resources that may be encountered during earthmoving activities, and the procedures to be followed if such resources are encountered.

- **Enforcement Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction
- **Monitoring Frequency:** Prior to issuance of demolition permit
- **Action Indicating Compliance:** Issuance of demolition permit

CUL-MM-2: In the event that historic (e.g., bottles, foundations, refuse dumps/privies, railroads, etc.) or prehistoric (e.g., hearths, burials, stone tools, shell and faunal bone remains, etc.) archaeological resources are unearthed, ground-disturbing activities shall be halted or diverted away from the vicinity of the find so that the find can be evaluated. An appropriate buffer area shall be established by the qualified Archaeologist around the find where construction activities shall not be allowed to continue. Work shall be allowed to continue outside of the buffer area. All archaeological resources unearthed by Project construction activities shall be evaluated by the qualified Archaeologist. If a resource is determined by the qualified Archaeologist to constitute a “historical resource” pursuant to CEQA Guidelines Section 15064.5(a) or a “unique archaeological resource” pursuant to PRC Section 21083.2(g), the qualified Archaeologist shall coordinate with the Applicant and the City to develop a formal treatment plan that would serve to reduce impacts to the resources. The treatment plan established for the resources shall be in accordance with CEQA Guidelines Section 15064.5(f) for historical resources and PRC Sections 21083.2(b) for unique archaeological resources. Preservation in place (i.e., avoidance) is the preferred manner of treatment. If preservation in place is not feasible, treatment may include implementation of archaeological data recovery excavations to remove the resource along with subsequent laboratory processing and analysis. Any archaeological material collected shall be curated at a public, non-profit institution with a research interest in the materials, such as the Fowler Museum, if such an institution agrees to accept the material. If no institution accepts the archaeological material, they shall be donated to a local school or historical society in the area for educational purposes.

- **Enforcement Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety

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- **Monitoring Phase:** Construction
- **Monitoring Frequency:** At time of resource discovery, should it occur
- **Action Indicating Compliance:** Compliance report by Qualified Archaeologist

CUL-MM-3: Prior to the release of the grading bond, the qualified Archaeologist shall prepare a final report and appropriate California Department of Parks and Recreation Site Forms for each resource at the conclusion of archaeological monitoring. The report shall include a description of resources unearthed, if any, treatment of the resources, results of the artifact processing, analysis, and research, and evaluation of the resources with respect to the California Register of Historical Resources and CEQA. The report and the Site Forms shall be submitted by the Applicant to the City of Los Angeles, the South Central Coastal Information Center, and representatives of other appropriate or concerned agencies to signify the satisfactory completion of the Project and required mitigation measures.

- **Enforcement Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** Once after completion of grading/excavation activities
- **Action Indicating Compliance:** Compliance report by Qualified Archaeologist

c) Geology and Soils

(1) Project Design Features

No project design features are identified in the EIR for this environmental issue.

(2) Mitigation Measures

GEO-MM-1: A Qualified Paleontologist meeting the Society for Vertebrate Paleontology (SVP) Standards (Qualified Paleontologist) shall be retained prior to the approval of demolition or grading permits. The Qualified Paleontologist shall provide technical and compliance oversight of all work as it relates to paleontological resources, shall attend the Project kick-off meeting and Project progress meetings on a regular basis, and shall report to the Project Site in the event potential paleontological resources are encountered.

- **Enforcement Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction

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- **Monitoring Frequency:** Prior to issuance of demolition or grading permit; Periodic during construction activities
- **Action Indicating Compliance:** Issuance of demolition or grading permit; Compliance report by Qualified Paleontologist

GEO-MM-2: The Qualified Paleontologist shall conduct construction worker paleontological resources sensitivity training at the Project kick-off meeting prior to the start of ground disturbing activities (including vegetation removal, pavement removal, etc.). In the event construction crews are phased, additional training shall be conducted for new construction personnel. The training session shall focus on the recognition of the types of paleontological resources that could be encountered within the Project Site and the procedures to be followed if they are found. Documentation shall be retained by the Qualified Paleontologist demonstrating that the appropriate construction personnel attended the training.

- **Enforcement Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction
- **Monitoring Frequency:** Prior to issuance of demolition or grading permit
- **Action Indicating Compliance:** Issuance of demolition or grading permit

GEO-MM-3: Paleontological resources monitoring shall be performed by a qualified paleontological monitor (meeting the standards of the SVP, 2010) under the direction of the Qualified Paleontologist. Paleontological resources monitoring shall be conducted for all ground disturbing activities in previously undisturbed sediments which have high sensitivity for encountering paleontological resources. Depending on the conditions encountered, full-time monitoring can be reduced to part-time inspections or ceased entirely if determined adequate by the Qualified Paleontologist. The Qualified Paleontologist shall spot check the excavation on an intermittent basis and recommend whether the depth of required monitoring needs to be revised based on his/her observations. Monitors shall have the authority to temporarily halt or divert work away from exposed fossils or potential fossils. Monitors shall prepare daily logs detailing the types of activities and soils observed and any discoveries.

If construction or other Project personnel discover any potential fossils during construction, regardless of the depth of work or location, work at the discovery location shall cease in a 50-foot radius of the discovery until the Qualified Paleontologist has assessed the discovery, conferred with the City, and made recommendations as to the appropriate treatment. Any significant fossils collected during Project-related excavations shall be prepared to the point of identification and curated into an accredited repository with retrievable storage, such as the Natural History Museum of Los Angeles County. The Qualified Paleontologist shall prepare a final monitoring and

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mitigation report for submittal to the City in order to document the results of the monitoring effort and any discoveries. If there are significant discoveries, fossil locality information and final disposition shall be included with the final report, which shall be submitted to the appropriate repository and the City.

- **Enforcement Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** Periodic
- **Action Indicating Compliance:** Compliance report by Qualified Paleontologist

d) Greenhouse Gas Emissions

(1) Project Design Features

See TRAF-PDF-1 (Transportation Demand Management Program) below.

(2) Mitigation Measures

No mitigation measures are identified in the EIR for this environmental issue.

e) Land Use and Planning

(1) Project Design Features

See Project Design Feature POL-PDF-2 (Security Features During Operation), TRAF-PDF-1 (Transportation Demand Management Program), and TRAF-PDF-2 (Construction Traffic Management Plan), below.

(2) Mitigation Measures

No mitigation measures are identified in the EIR for this environmental issue.

f) Noise

(1) Project Design Features

NOI-PDF-1: Impact Pile Driving and Blasting Prohibitions. The Project will not use or allow impact pile drivers and will not require or allow blasting during construction activities. Augured or drilled piles are allowed.

- **Enforcement Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction

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- **Monitoring Frequency:** Periodic field inspections
- **Action Indicating Compliance:** Field inspection sign-off

(2) Mitigation Measures

NOI-MM-1: The Project shall provide temporary ground-level construction noise barriers, with a minimum height of eight feet along Orange Street to the north, South San Vicente to the west, South Sweetzer Avenue to the south, and a minimum height of 15 feet along the alleyway to the northeast/east, equipped with noise blankets or equivalent noise reduction materials rated to achieve sound level reductions of at least 10 dBA between the Project Site and ground-level sensitive receptor locations. These temporary noise barriers shall be used to block the line-of-sight between the construction equipment and the noise-sensitive receptor(s) during the duration of construction activities. Prior to obtaining any permits, documentation prepared by a noise consultant verifying compliance with this measure shall be submitted to the Department of City Planning.

- **Enforcement Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** Periodic field inspections
- **Action Indicating Compliance:** Field inspection sign-off

NOI-MM-2: Noise- and vibration-generating construction equipment whose specific location on the Project Site may be flexible (e.g., compressors and generators) shall be located away from the nearest off-site sensitive land uses (at least 100 feet away), or natural and/or manmade barriers (e.g., intervening construction trailers) shall be used to screen propagation of noise from such equipment towards these land uses.

- **Enforcement Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** Periodic field inspections
- **Action Indicating Compliance:** Field inspection sign-off

NOI-MM-3: The Project contractor shall use power construction equipment with state-of-the-art noise shielding and muffling devices. Flexible sound control curtains shall be placed around all drilling apparatuses, drill rigs, and jackhammers when in use that shall achieve a sound level reduction of at least 10 dBA between the Project Site and ground-level sensitive receptor locations.

- **Enforcement Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** During construction

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- **Monitoring Frequency:** Ongoing during Project operation
- **Action Indicating Compliance:** Field inspection signoff

NOI MM-4: A construction liaison shall be provided to inform the nearby receptors when peak noise and vibration activities are scheduled to occur. Two weeks prior to the commencement of construction at the Project Site, notification shall be provided to properties identified as sensitive receptors that discloses the construction schedule, including the various types of activities and equipment that would be occurring throughout the duration of the construction period.

- **Enforcement Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at Project plan check prior to building permit; Periodic field inspections
- **Action Indicating Compliance:** Plan approval and issuance of applicable building permit; Field inspection signoff

g) Fire Protection

(1) Project Design Features

See Project Design Features TRAF-PDF-2 (Construction Traffic Management Plan) and TRAF-PDF-3 (Construction Worker Parking Plan), below.

(2) Mitigation Measures

No mitigation measures are identified in the EIR for this environmental issue.

h) Police Protection

(1) Project Design Features

See Project Design Features TRAF-PDF-2 (Construction Traffic Management Plan) and TRAF-PDF-3 (Construction Worker Parking Plan), below.

POL-PDF-1: Security Features During Construction. During construction, the Project Site shall be fenced and gated with surveillance cameras to monitor the site during off hours. Security lighting shall also be provided in and around the construction site.

- **Enforcement Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** Periodic field inspections

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- **Action Indicating Compliance:** Field inspection sign-offs

POL-PDF-2: Security Features During Operation. During operation of the Project, access to the parking structure shall be controlled through gated entries, and the entry areas shall be well illuminated. Project Site security shall include controlled keycard access to medical office spaces, security lighting within common areas and entryways, and closed circuit TV monitoring (CCTV).

- **Enforcement Agency:** City of Los Angeles Department of City Planning, City of Los Angeles Department of Building and Safety
- **Monitoring Agency:** City of Los Angeles Department of City Planning, City of Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Once prior to issuance of Certificate of Occupancy
- **Action Indicating Compliance:** Plan approval and issuance of applicable building permit; Issuance of Certificate of Occupancy

(2) Mitigation Measures

No mitigation measures are identified in the EIR for this environmental issue.

i) Transportation

(1) Project Design Features

TRAF-PDF-1: Transportation Demand Management (TDM) Program. The Applicant will implement a TDM Program aimed at discouraging single-occupancy vehicle trips and encouraging alternative modes of transportation, such as carpooling, taking transit, walking, and biking. The TDM Program will be subject to review and approval by the Los Angeles Department of City Planning and LADOT. The exact measures to be implemented will be determined when the Program is prepared, prior to issuance of a final certificate of occupancy for the Project. The strategies will include, at a minimum, the following:

- Bicycle facilities including short-term and long-term parking, and onsite lockers and showers in accordance with Planning Code requirements; and
- Marketing and promotions, including a transportation information center, kiosks and/or other on-site measures, such as providing a Tenant Welcome Package (i.e., all new commercial tenants receive information on available alternative modes and ways to access destinations).

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- **Enforcement Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Transportation
- **Monitoring Agency:** City of Los Angeles Department of City Planning; City of Los Angeles Department of Transportation
- **Monitoring Phase:** Pre-construction; Construction; Operation
- **Monitoring Frequency:** Once prior to issuance of building permit; Once prior to issuance of Certificate of Occupancy; Periodic field inspections during operation
- **Action Indicating Compliance:** Approval of TDM Program from the City of Los Angeles Department of City Planning and Los Angeles Department of Transportation prior to issuance of building permit; Issuance of Certificate of Occupancy; Field inspection sign-offs

TRAF-PDF-2: Construction Traffic Management Plan. Prior to the issuance of a building permit for the Project, a detailed Construction Traffic Management Plan (CTMP), including street closure information, a detour plan, haul routes, and a staging plan, will be prepared and submitted to the City for review and approval. The CTMP will formalize how construction will be carried out and identify specific actions that will be required to reduce effects on the surrounding community. The CTMP will be based on the nature and timing of the specific construction activities and other projects in the vicinity of the Project Site. Construction management meetings with City staff and other surrounding construction-related project representatives (i.e., construction contractors), whose projects will potentially be under construction at around the same time as the Project, will be conducted bimonthly, or as otherwise determined appropriate by City staff. This coordination will ensure construction activities of the concurrent related projects and associated hauling activities are managed in collaboration with one another and the Project. The CTMP will include, but not be limited to, the following elements as appropriate:

- Advance notification of adjacent property owners and occupants, as well as nearby schools, of upcoming construction activities, including durations and daily hours of construction.
- As parking lane and/or travel lane closures are anticipated, worksite traffic control plan(s), approved by the City of Los Angeles, should be implemented to route vehicular traffic, bicyclists, and pedestrians around any such closures.
- Safety precautions for pedestrians and bicyclists through such measures as alternate routing and protection barriers, as appropriate.
- Schedule deliveries and pick-ups of construction materials during non-peak travel periods to the extent possible and coordinate to reduce the potential of trucks waiting to load or unload for protracted periods.
- Provide off-site truck staging in a legal area furnished by the construction truck contractor. Anticipated truck access to the Project Site will be off of the South San Vicente Boulevard frontage road.

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- Scheduling of construction activities to reduce the effect on traffic flow on surrounding arterial streets.
 - Advanced notification of temporary on-street parking removals and duration of removals along the South San Vicente Boulevard frontage road and Orange Street.
 - Coordinate with the City and emergency service providers to ensure adequate access, including emergency access, is maintained to the Project Site and neighboring businesses and residences. Emergency access points will be marked accordingly in consultation with the Los Angeles Fire Department (LAFD), as necessary.
- **Enforcement Agency:** City of Los Angeles Department of Transportation
 - **Monitoring Agency:** City of Los Angeles Department of Transportation
 - **Monitoring Phase:** Pre-construction; Construction
 - **Monitoring Frequency:** Once prior to issuance of building permit; Periodic field inspections
 - **Action Indicating Compliance:** Approval of Construction Traffic Management Plan from the Los Angeles Department of Transportation prior to issuance of building permit; Field inspection sign-offs

TRAF-PDF-3: Construction Worker Parking Plan. The Applicant will prepare a Construction Worker Parking Plan prior to commencement of construction to identify and enforce parking location requirements for construction workers. The Construction Worker Parking Plan will include, but not be limited to, the following elements as appropriate:

- During construction activities when construction worker parking cannot be accommodated on the Project Site, the plan will identify alternate parking location(s) for construction workers and the method of transportation to and from the Project Site (if beyond walking distance) for approval by the City 30 days prior to commencement of construction.
 - Construction workers will not be permitted to park on street.
 - All construction contractors will be provided with written information on where their workers and their subcontractors are permitted to park and provide clear consequences to violators for failure to follow these regulations.
- **Enforcement Agency:** City of Los Angeles Department of Transportation
 - **Monitoring Agency:** City of Los Angeles Department of Transportation
 - **Monitoring Phase:** Pre-construction; Construction
 - **Monitoring Frequency:** Once prior to issuance of building permit; Periodic field inspections
 - **Action Indicating Compliance:** Approval of Construction Worker Parking Plan from the Los Angeles Department of Transportation prior to issuance of building permit; Field inspection signoffs

(2) Mitigation Measures

No mitigation measures are identified in the EIR for this environmental issue.

MEMORANDUM

TO: Bryan Fairbanks, Stockdale Capital Partners, LLC

FROM: Sarah M. Drobis, P.E., and Casey Le, P.E.

DATE: January 4, 2022

RE: Supplemental Parking Analysis for the
656 South San Vicente Medical Office Project
Los Angeles, California

Ref: J1534

Gibson Transportation Consulting, Inc. was asked to provide supplemental parking information and analysis for the 656 South San Vicente Medical Office Project (Project). The supplemental analysis includes an evaluation of applicable parking rates for the Project's proposed land use types, including review of empirical parking demand data collected at medical office uses, and resulting peak parking demand. This memorandum summarizes our analysis.

PROJECT BACKGROUND

The Project includes a 140,305 square foot (sf) medical office building and approximately 5,000 sf of commercial space, including 4,000 sf of restaurant use and 1,000 sf of retail/pharmacy use. The on-site building that formerly operated as a private school, which was vacated in October 2018, and the existing 8,225 sf sporting goods store and associated surface parking lot would be removed to accommodate the Project. The Project is anticipated to be completed by Year 2023. The Project site is located at 656 South San Vicente Boulevard in the Wilshire Community Plan area of the City of Los Angeles (City).

A total of 418 vehicular parking spaces would be provided in four above-grade parking levels with full valet operations. Additionally, up to 33 additional parking spaces, for a total of 451 spaces, could be accommodated through unstriped aisle, tandem, and other parking spaces with full valet operations within three of the four parking levels. Visitor access to the drop-off and valet area on the ground level of the parking garage would be accommodated via separate ingress and egress visitor-only driveways along the San Vicente Boulevard frontage road. Employee access to the drop-off and valet area on the second level would be provided via one employee-only driveway along Orange Street. The Project would also provide a total of 716 bicycle parking spaces on-site, including 18 short-term and 698 long-term spaces. The short-term bicycle parking spaces would be provided on the ground level; the long-term bicycle parking spaces would be provided on the rooftop level with full valet operations. The Project site plan is shown in Figure 1.

The Project would implement a transportation demand management (TDM) program to reduce single-occupancy vehicle trips and parking demand to the Project site. The TDM program would include strategies such as education and marketing to encourage employees and visitors to utilize alternative transportation modes (e.g., transit, bus, walking, bicycling, carpool, vanpool, etc.), provision of bicycle and pedestrian amenities, and support for carpools and rideshares. The Project's TDM program would be subject to review and approval by the City. The Project would also implement a parking management plan that would include strategies such as TDM measures to reduce parking demand and full attendant-operated stacked parking to increase the parking supply, as detailed above.

CODE PARKING ANALYSIS

The parking requirements of the Project were calculated by applying the applicable parking ratios from Los Angeles Municipal Code (LAMC) Section 12.21A.4.(c) for commercial uses and Section 12.21A.4.(d) for medical office uses. The LAMC parking rates detailed in Table 1 were applied to the Project and resulted in a total baseline parking requirement of 746 parking spaces. Pursuant to Section 12.32.P of the LAMC, the Project is requesting a reduction in parking not to exceed 20%, incident to a legislative action, of the required baseline City code parking requirements. Additionally, per Section 12.21.A4 of the LAMC, a non-residential building may replace up to 20% of its required vehicle parking spaces with bicycle parking at a ratio of four bicycle parking spaces to one vehicle parking space. Furthermore, for projects located within 1,500 feet of a major transit station (in the case of the Project, the future Los Angeles County Metropolitan Transportation Authority [Metro] D Line Wilshire/La Cienega Station would be located 1,500 feet west of the Project site), up to 30% of the non-residential vehicle parking may be replaced with bicycle parking. Thus, as detailed in Table 1, the total LAMC-required vehicle parking after reductions is 418 spaces. The Project parking supply of 418 striped stalls and 33 unstriped stalls would accommodate the LAMC off-street parking requirements.

PARKING DEMAND ANALYSIS

Code parking requirements are not necessarily reflective of the parking demands experienced with a development as a whole as they represent the sum of the peak parking requirements for individual land uses and do not account for the parking demand or shared parking concept (i.e., the hourly and/or day of the week variations in parking demand generated by individual land uses), nor for the synergy between uses. To provide further information, an evaluation of the potential peak parking demand pattern was prepared for the Project.

Shared Parking Methodology

The assessment of the parking demand for a mixed-use project is accomplished through the calculation of shared parking demand for the overall site, with each land use's parking demand pattern added together for each hour of the day.

As part of their national research on shared parking, the International Council of Shopping Centers (ICSC), the Urban Land Institute (ULI), and the National Parking Association (NPA) developed a database that identifies the peak parking demand for every land use typically found

within a mixed-use development. This national research database forms the basis for the assumptions in the shared parking model. *Shared Parking, 3rd Edition* (ULI, ICSC and NPA, February 2020) describes shared parking as follows:

“Shared parking is defined as parking space that can be used to serve two or more individual land uses without conflict or encroachment. The opportunity to implement shared parking is the result of two conditions:

- Variations in the peak accumulation of parked vehicles as the result of different activity patterns of adjacent or nearby land uses (by hour, by day, by season)
- Relationships among land use activities that result in people’s attraction to two or more land uses on a single auto trip to a given area or development”

Most zoning codes provide peak parking ratios for individual land uses. While this appropriately recognizes that separate land uses generate different parking demands on an individual basis, it does not reflect the fact that the combined peak parking demand, when a mixture of land uses shares the same parking supply, can be substantially less than the sum of the individual demands. For example, retail uses experience peak demand in the early to mid-afternoon, while restaurant uses experience peak demand in the lunchtime and/or evening hours (depending on the type of restaurant).

While it should be noted that the Project contains one primary land use, the medical office, a shared parking model was used to determine the parking demand rates and hourly distribution patterns of all proposed uses on-site, including the restaurant and retail/pharmacy uses.

Model Calibration Methodology

Shared Parking, 3rd Edition defines national averages to be used as parking demand rates for various land uses and it suggests ranges of assumptions regarding transit and internal capture to be used. The methodology states that the best way to measure the demand at a particular site is to use local data to modify the national averages so that it reflects local conditions. The shared parking model may be modified to use local California conditions in place of national averages when local data is available. As detailed above, a shared parking model was prepared and calibrated to the anticipated operations of the Project.

Empirical Parking Data

In accordance with *Shared Parking, 3rd Edition*, an empirical parking demand rate for the Project’s medical office use was developed based on a review of local empirical parking demand data collected at three medical office sites located in the cities of Santa Monica and Beverly Hills. The sites were selected based on their similar characteristics to the Project, including medical service type and proximity to available transit options. Parking occupancy surveys for monthly and transient parkers were conducted at the sites during typical weekdays from January to February 2020. The peak parking demand rates were calculated based on the peak number of occupied parking spaces divided by the total floor area for each site and are summarized in the Attachment.

Based on a review of the three sites, the empirical parking demand rate for the medical office located at 9090 Wilshire Boulevard was selected for use in this analysis as it is located approximately one mile west of the Project site and serviced by numerous bus lines, as well as the future Metro D Line rail transit, similar to the Project. Based on the parking occupancy surveys for the 9090 Wilshire Boulevard site, the total weekday peak parking rate is 3.43 spaces per 1,000 sf, including a peak visitor parking rate of 1.76 spaces per 1,000 sf and a peak employee parking rate of 1.67 spaces per 1,000 sf. Details of the empirical parking demand rate development is provided in the Attachment.

Model Adjustment Factors

The following discussion details the adjustment factors available for use in the shared parking model and describes the basis for the adjustment of these factors.

Time of Day. The time-of-day factor is one of the key assumptions of the shared parking model. This factor reveals the hourly parking pattern of the analyzed land use; essentially, the peak demands of the mixed-use project are calculated using these factors. The *Shared Parking, 3rd Edition* research efforts have yielded a comprehensive data set of time-of-day factors for multiple land uses. As the demand for each land use fluctuates over the course of the day, the ability to implement shared parking emerges. No time-of-day factor adjustments were made for the Project.

Weekday vs. Weekend Parking Ratio. The shared parking model measures the parking demand on a weekday as well as on a Saturday. *Shared Parking, 3rd Edition* indicates that a source for variation in parking demand can be traced to the difference between weekday and weekend demand. This variation is typically seen in the parking demand rates of the model.

The *Shared Parking, 3rd Edition* methodology requires that each land use select parking ratios; that is, the parking ratio for each land use if used independently. The base parking demand rates for visitors and employees to the medical office uses were based on the empirical parking demand rate detailed above. The base parking demand rates for the retail/pharmacy and restaurant uses were from *Shared Parking, 3rd Edition* and LAMC, respectively, were directly applied to the model. Thus, the following peak parking demand rates for weekday and weekend conditions were used in the model:

<u>Land Use</u>	<u>Unit</u>	<u>Parking Ratios (Visitor / Employee)</u>	
		Weekday	Weekend
Medical Office	spaces/1,000 sf	1.76 / 1.67	N/A ¹
Retail/Pharmacy	spaces/1,000 sf	3.00 / 0.40	3.00 / 0.40
Restaurant	spaces/1,000 sf	8.60 / 1.40	8.60 / 1.40

Seasonal Variation. Seasonal variations used in the model were derived from *Shared Parking, 3rd Edition* average rates. The shared parking analysis in this report was based on the peak

¹ Consistent with the assumptions in *Shared Parking, 3rd Edition*, no parking demand was assumed during a typical weekend for the medical office use.

month of the year. The total parking demand was compared over the course of the year and the peak month's demand is reported. No seasonal variation adjustments were made for the Project.

Mode Split. Another factor that affects the overall parking demand at a development is the number of employees and visitors that arrive by automobile compared to other means (transit, walk, shared car service, etc.) Based on the effectiveness of the Project's proposed TDM program and the surrounding transit options, including the future Metro D Line Wilshire/La Cienega Station, a 15% mode-split adjustment was applied to account for visitors and employees envisioned to walk-in from adjacent neighborhoods and commercial uses and/or take transit. The mode split adjustment also accounts for a growing number of visitors and employees who are anticipated to utilize rideshare services (e.g., Uber, Lyft, etc.) to travel to and from the Project site.

Captive Market. It is common that a mixed-use project has patrons/visitors captured by other uses within the site itself. The shared parking model accounts for the synergy of uses for a typical weekday and weekend for the Project. No further adjustments were made to the internal capture assumptions of the model.

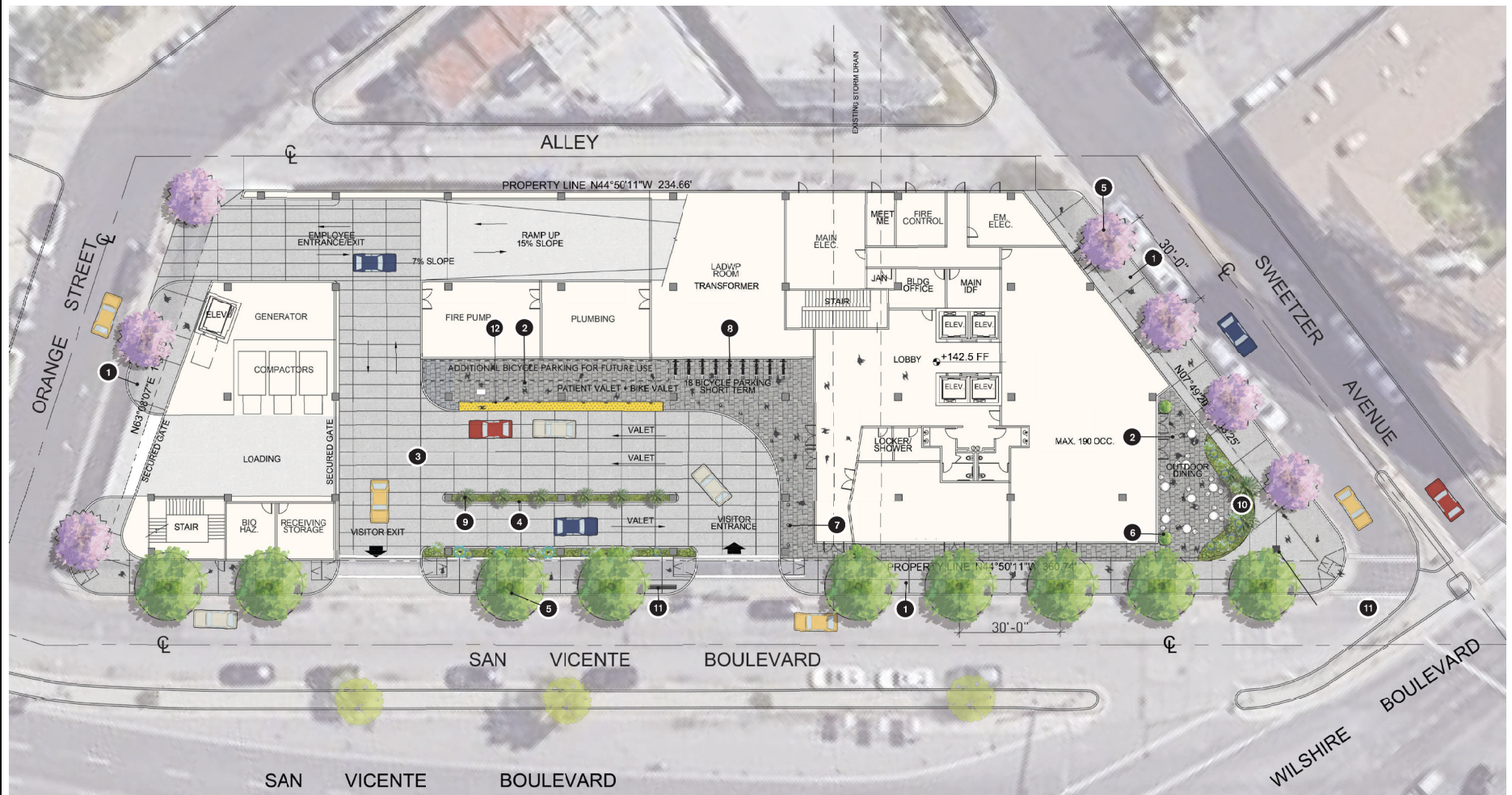
Auto Occupancy. This shared parking analysis used the *Shared Parking, 3rd Edition* national averages for automobile occupancy for all land uses. No adjustments were made to the average rates.

Parking Demand Projections

Table 1 and Figure 2 summarize the Project's combined hourly peak parking demand results for a typical weekday and weekend. Figure 3 provides a detailed hourly parking demand for each land use type during the peak weekday. As detailed, the peak parking demand is projected to occur at 11 AM and 2 PM on a weekday, with a peak demand of 422 spaces (217 visitor spaces and 205 employee spaces), and at 12 PM on a weekend, with a peak demand of 37 spaces (31 visitor spaces and six employee spaces). The Project parking supply would be able to accommodate the Project's parking demands throughout the day for both weekday and weekend conditions.

SUMMARY

As detailed above, the Project would implement a combination of TDM strategies and parking management strategies, including full-time attendant-operated stacked parking, as part of its TDM program. The peak parking demand for the Project would total approximately 422 spaces. Thus, the Project's parking supply of 451 parking spaces, including 418 parking spaces and up to 33 additional parking spaces through unstriped aisle, tandem, and other parking spaces, would be able to accommodate the anticipated peak parking demand during both the weekday and weekend.



Source: ZGF Architects. November, 2021.

PROJECT SITE PLAN

FIGURE
1

**TABLE 1
VEHICLE PARKING CODE REQUIREMENTS**

CITY CODE PARKING REQUIREMENTS [a]			
Land Use	Size	Parking Rate	Total Spaces
Medical Office Buildings	140,305 sf	5.00 sp / 1,000 sf	702
Retail Stores, General	1,000 sf	4.00 sp / 1,000 sf	4
Restaurant and Bars, General	4,000 sf	10.00 sp / 1,000 sf	40
Total Baseline Parking Requirement			746
<u>Parking Reduction per Request [b]</u>			
<i>Total Project</i>		20%	(149)
<u>Parking Reduction per Bicycle Replacement [c]</u>			
<i>Total Project (Non-Residential) - 30%</i>	716 sp	1 sp / 4 sp	(179)
Total Code Parking Requirement w/ Reductions			418
Total Parking Provided [d]			451

Notes:

[a] Parking rates per LAMC Section 12.21. A4 (c) for commercial uses and Section 12.21. A4 (d) for medical office uses.

[b] Per Section 12.32.P of the LAMC, the Project is requesting a reduction in parking not to exceed 20%, incident to a legislative action, of the required baseline City code parking requirements.

[c] Per Section 12.21.A4 of the LAMC, non-residential buildings may replace up to 20% of the required vehicle parking with bicycle parking at a ratio of four bicycle parking spaces to one vehicle parking space. Furthermore, non-residential buildings located within 1,500 feet of a major transit stop may replace up to 30% of the required vehicle parking with bicycle parking. The Project is located within 1,500 feet from the future Metro D Line Wilshire/La Cienega Station.

[d] The Project would provide a total of 451 parking spaces, including 418 parking spaces and up to 33 additional parking spaces through unstriped aisle, tandem and other parking spaces.

**TABLE 2
PARKING DEMAND SUMMARY FOR
656 S. SAN VICENTE MEDICAL OFFICE PROJECT**

Shared Parking Demand Summary																		
Peak Month: MARCH -- Peak Period: 2 PM, WEEKDAY																		
Land Use	Project Data		Weekday					Weekend					Weekday			Weekend		
			Base Ratio	Driving Adj	Non-Captive Ratio	Project Ratio	Unit For Ratio	Base Ratio	Driving Adj	Non-Captive Ratio	Project Ratio	Unit For Ratio	Peak Hr Adj 2 PM	Peak Mo Adj March	Estimated Parking Demand	Peak Hr Adj 12 PM	Peak Mo Adj May	Estimated Parking Demand
Retail																		
Retail/Pharmacy Employee	1,000	sf GLA	3.00 0.40	85% 85%	58% 100%	1.47 0.34	ksf GLA	3.00 0.40	85% 85%	99% 100%	2.52 0.34	ksf GLA	95% 100%	92% 100%	1 1	100% 100%	91% 100%	2 1
Food and Beverage																		
Restaurant Employee	4,000	sf GLA	8.60 1.40	85% 85%	22% 100%	1.59 1.19	ksf GLA	8.60 1.40	85% 85%	96% 100%	7.05 1.19	ksf GLA	90% 95%	97% 100%	6 5	100% 100%	99% 100%	29 5
Entertainment and Institutions																		
Hotel and Residential																		
Office																		
Medical Office Employee	140,305	sf GFA	1.76 1.67	85% 85%	100% 100%	1.49 1.42	ksf GFA	0.00 0.00	85% 85%	100% 100%	0.00 0.00	ksf GFA	100% 100%	100% 100%	210 199	30% 100%	100% 100%	- -
Additional Land Uses																		
													Customer/Visitor		217	Customer		31
													Employee/Resident		205	Employee/Resident		6
													Reserved		-	Reserved		-
													Total		422	Total		37

FIGURE 2
PEAK MONTH PARKING DEMAND BY HOUR (WEEKDAY/WEEKEND)
656 S. SAN VICENTE MEDICAL OFFICE PROJECT

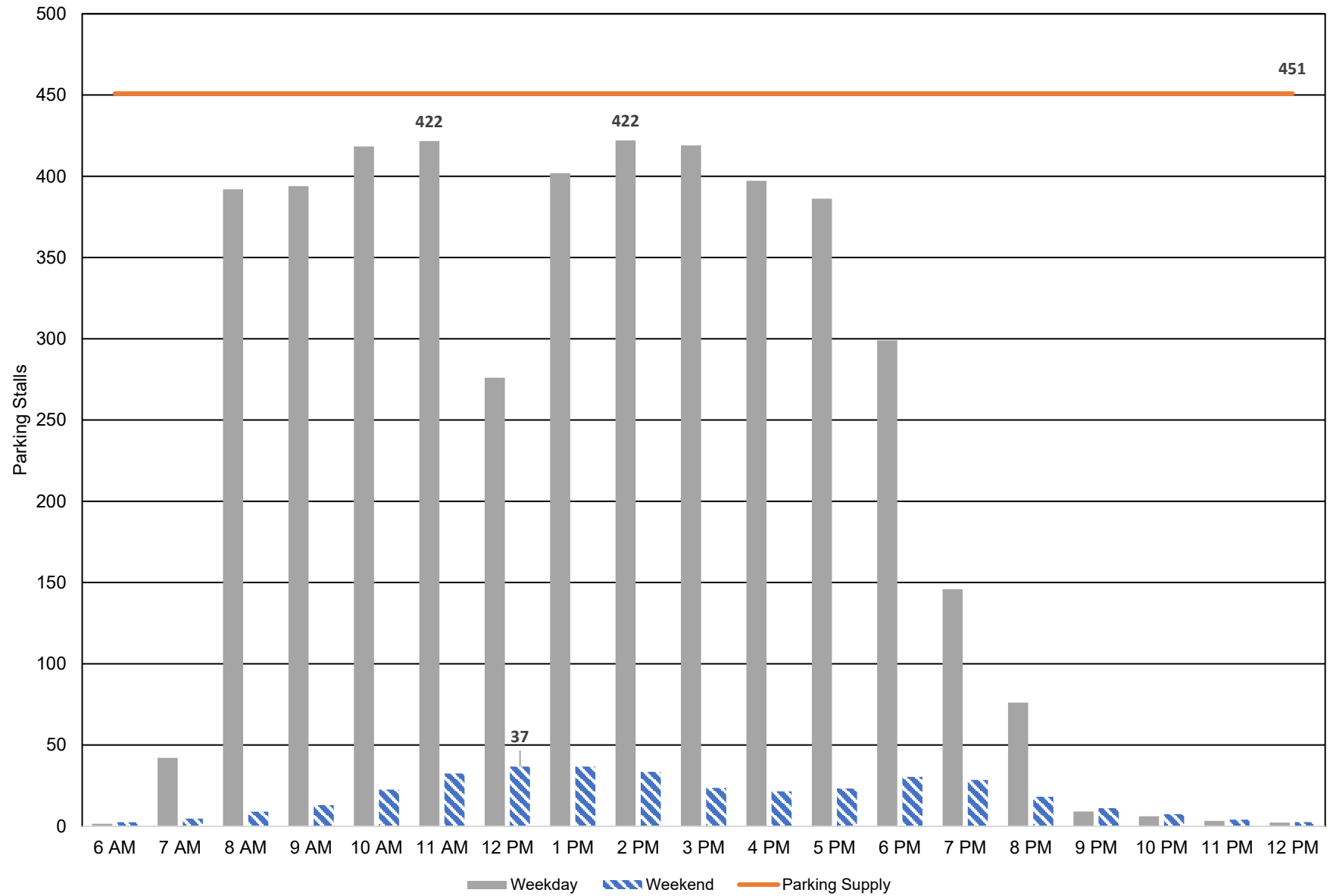
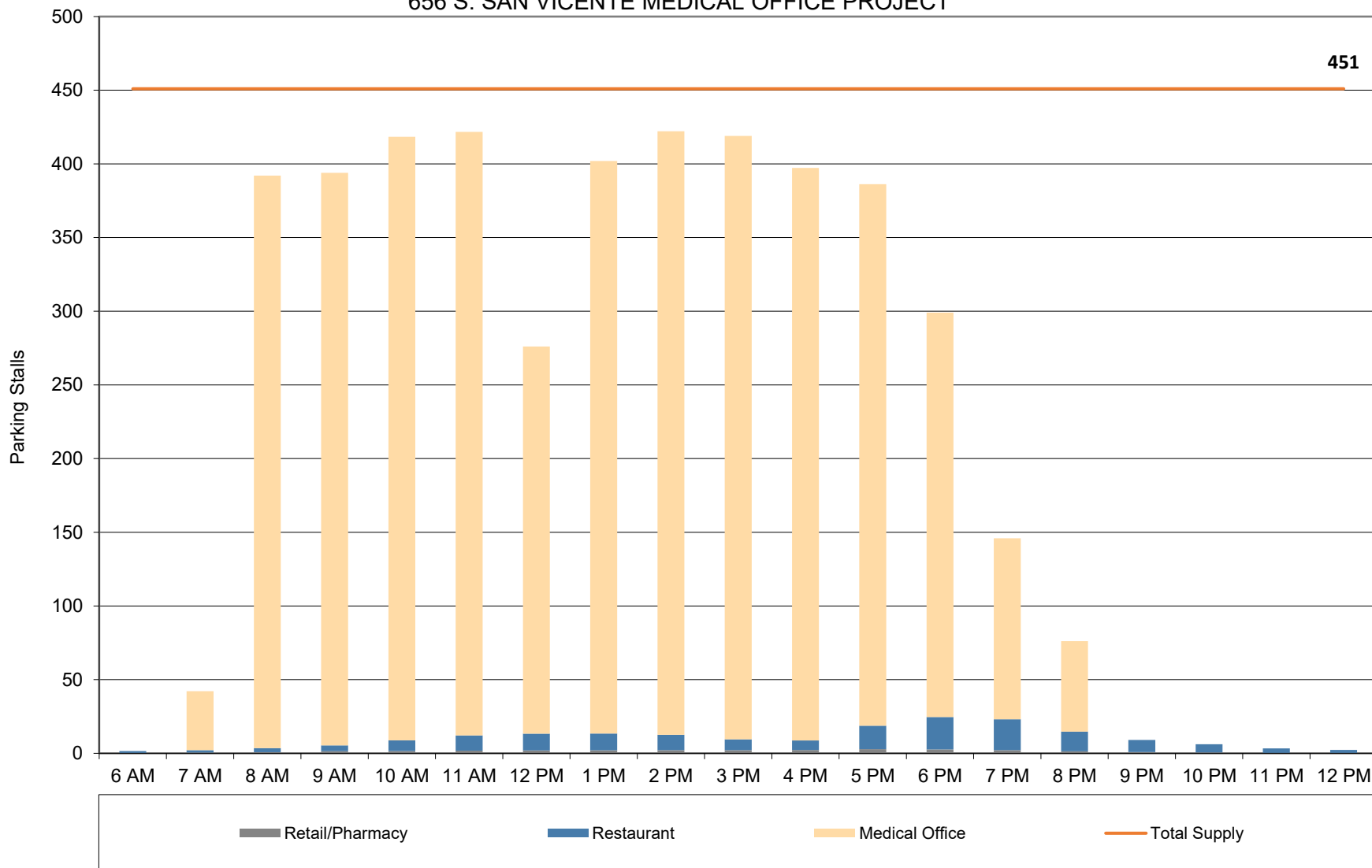


FIGURE 3
PEAK MONTH PARKING DEMAND BY HOUR BY LAND USE (WEEKDAY ONLY)
656 S. SAN VICENTE MEDICAL OFFICE PROJECT



Attachment

**ATTACHMENT
LOCAL MEDICAL OFFICE SITES
PARKING DEMAND RATE COMPARISON**

Medical Office Site	Location	Transit Availability	Floor Area (sf)	Weekday Parking Demand (sp) [a]	Peak Parking Demand Rate (sp/1,000 sf)
2811 Wilshire	Santa Monica	Yes	97,228	241	2.48
9090 Wilshire Blvd	Beverly Hills	Yes	51,570	177	3.43
2825 Santa Monica Blvd	Santa Monica	Yes	54,246	113	2.08

Notes:

[a] The parking occupancy observed at a majority of the sites was between 78-96%, which is considered fully occupied.

DRAFT

MEMORANDUM

TO: Bryan Fairbanks, Stockdale Capital Partners, LLC

FROM: Sarah M. Drobis, P.E., and Casey Le, P.E.

DATE: January 31, 2022

RE: Supplemental Parking Analysis for the
656 South San Vicente Medical Office Project
Los Angeles, California

Ref: J1534

Gibson Transportation Consulting, Inc. (GTC) was asked to provide supplemental parking analyses for the 656 South San Vicente Medical Office Project (Project) based on the incorporation of medical laboratory space into the medical office floor area (Refined Project). The parking analysis herein was prepared consistent with the methodology, assumptions, and analysis detailed in *Supplemental Parking Analysis for the 656 South San Vicente Medical Office Project, Los Angeles, California* (GTC, January 4, 2022) (Supplemental Parking Memorandum).

REFINED PROJECT DESCRIPTION

Consistent with the Project, the Refined Project proposes 140,305 square foot (sf) of medical office, consisting of 28,061 sf of medical laboratory space (approximately 20% of the total) and 112,244 sf medical office space, and approximately 5,000 sf of commercial restaurant and retail/pharmacy uses. Consistent with the Project, the Refined Project would provide a total of 418 vehicular parking spaces plus up to 33 additional parking spaces through unstriped aisle, tandem, and other parking spaces, for a total of 451 spaces, with full valet operations. No changes to the vehicular or pedestrian access are proposed under the Refined Project. In addition, the Refined Project would implement a transportation demand management (TDM) program to reduce single-occupancy vehicle trips and parking demand to the Project site and would implement a parking management plan.

PARKING DEMAND ANALYSIS

Consistent with the Supplemental Parking Memorandum, an evaluation of the potential peak parking demand pattern was prepared for the Refined Project utilizing a shared parking model. The base parking demand rates for visitors and employees of the medical office use were based on local empirical parking demand data. The base parking demand rates from *Shared Parking, 3rd Edition* (Urban Land Institute, International Council of Shopping Centers, and National Parking Association, February 2020) for the retail/pharmacy use and the Los

Angeles Municipal Code rates for medical laboratory and restaurant uses were directly applied to the model.

Consistent with the Supplemental Parking Memorandum, a mode split adjustment was also applied to the model to account for the proposed TDM program, future transit options, and rideshare services.

Parking Demand Projections

Table 1 summarizes the peak parking demand comparison between the Project and the Refined Project. Table 2 and Figure 1 summarize the Refined Project's combined hourly peak parking demand results for a typical weekday and weekend. Figure 2 provides a detailed hourly parking demand for each land use type during the peak weekday. As detailed, the peak parking demand for the Refined Project is projected to occur at 11 AM on a weekday, with a peak demand of 386 spaces (177 visitor spaces and 211 employee spaces), and at 12 PM on a weekend, with a peak demand of 37 spaces (31 visitor spaces and six employee spaces). By comparison, the incorporation of the medical laboratory space with the Refined Project results in less peak parking demand as compared to the Project. The parking supply would be able to accommodate the anticipated parking demands throughout the day for both weekday and weekend conditions.

SUMMARY

As detailed above, consistent with the Project, the Refined Project would implement a combination of TDM strategies and parking management strategies, including full-time attendant-operated stacked parking, as part of its TDM program. The peak parking demand for the Refined Project would total approximately 386 spaces, as compared to 422 spaces with the Project.

Thus, the parking supply of 451 parking spaces, including 418 parking spaces and up to 33 additional parking spaces through unstriped aisle, tandem, and other parking spaces, would be able to accommodate the anticipated peak parking demand during both the weekday and weekend.

**TABLE 1
PARKING COMPARISON SUMMARY**

Project [a]	Total Parking Provided [b]	Weekday Peak Parking Demand [c]	Surplus/Deficiency
<u>Proposed Project</u> 140,305 sf Medical-Office 5,000 sf Commercial	451	422	29
<u>Refined Project - 20% Medical Lab Space</u> 112,244 sf Medical Office 28,061 sf Medical Lab 5,000 sf Commercial	451	386	65

Notes:

[a] The Project proposes approximately 5,000 square feet (sf) of commercial space, including 4,000 sf restaurant and 1,000 sf retail/pharmacy.

[b] The Project would provide a total of 451 parking spaces, including 418 parking spaces and up to 33 additional parking spaces through unstriped aisle, tandem and other parking spaces.

[c] The peak parking demand represents the highest hour parking demand on a typical weekday. See subsequent tables and figures for the parking demand evaluation, which reflects the following assumptions:

- The peak parking demand rate for medical office (3.43 spaces per 1,000 sf) is based on local empirical parking demand data, as compared to the Code parking requirement (5.00 spaces per 1,000 sf);
- The peak parking demand rate for medical laboratory/research and development space is based on 2.0 spaces per 1,000 sf, which is consistent with the Code parking requirement;
- The parking demand analysis reflects the effectiveness of the Project's location to the future transit systems and implementation of a Transportation Demand Management program.

**TABLE 2
PARKING DEMAND SUMMARY FOR
656 S. SAN VICENTE MEDICAL OFFICE REFINED PROJECT (20% MEDICAL LAB SPACE)**

Shared Parking Demand Summary																		
Peak Month: MAY -- Peak Period: 11 AM, WEEKDAY																		
Land Use	Project Data		Weekday					Weekend					Weekday			Weekend		
			Base Ratio	Driving Adj	Non-Captive Ratio	Project Ratio	Unit For Ratio	Base Ratio	Driving Adj	Non-Captive Ratio	Project Ratio	Unit For Ratio	Peak Hr Adj	Peak Mo Adj	Estimated Parking Demand	Peak Hr Adj	Peak Mo Adj	Estimated Parking Demand
	Quantity	Unit											11 AM	May		12 PM	May	
Retail																		
Retail/Pharmacy Employee	1,000	sf GLA	3.00 0.40	85% 85%	57% 100%	1.46 0.34	ksf GLA	3.00 0.40	85% 85%	99% 100%	2.52 0.34	ksf GLA	67% 100%	91% 100%	1 1	100% 100%	91% 100%	2 1
Food and Beverage																		
Restaurant Employee	4,000	sf GLA	8.60 1.40	85% 85%	21% 100%	1.53 1.19	ksf GLA	8.60 1.40	85% 85%	96% 100%	7.05 1.19	ksf GLA	85% 100%	99% 100%	5 5	100% 100%	99% 100%	29 5
Entertainment and Institutions																		
Hotel and Residential																		
Office																		
Medical Office Employee	112,244	sf GFA	1.76 1.67	85% 85%	100% 100%	1.49 1.42	ksf GFA	0.00 0.00	85% 85%	100% 100%	0.00 0.00	ksf GFA	100% 100%	100% 100%	168 160	30% 100%	100% 100%	- -
Medical Lab Space Employee	28,061	sf GFA	0.16 1.84	85% 85%	74% 100%	0.10 1.57	ksf GFA	0.00 0.00	85% 85%	100% 100%	0.00 0.00	ksf GFA	45% 100%	100% 100%	2 44	90% 90%	100% 100%	- -
Additional Land Uses																		
													Customer/Visitor	176	Customer	31		
													Employee/Resident	210	Employee/Resident	6		
													Reserved	-	Reserved	-		
													Total	386	Total	37		

FIGURE 1
PEAK MONTH PARKING DEMAND BY HOUR (WEEKDAY/WEEKEND)
656 S. SAN VICENTE MEDICAL OFFICE REFINED PROJECT (20% MEDICAL LAB)

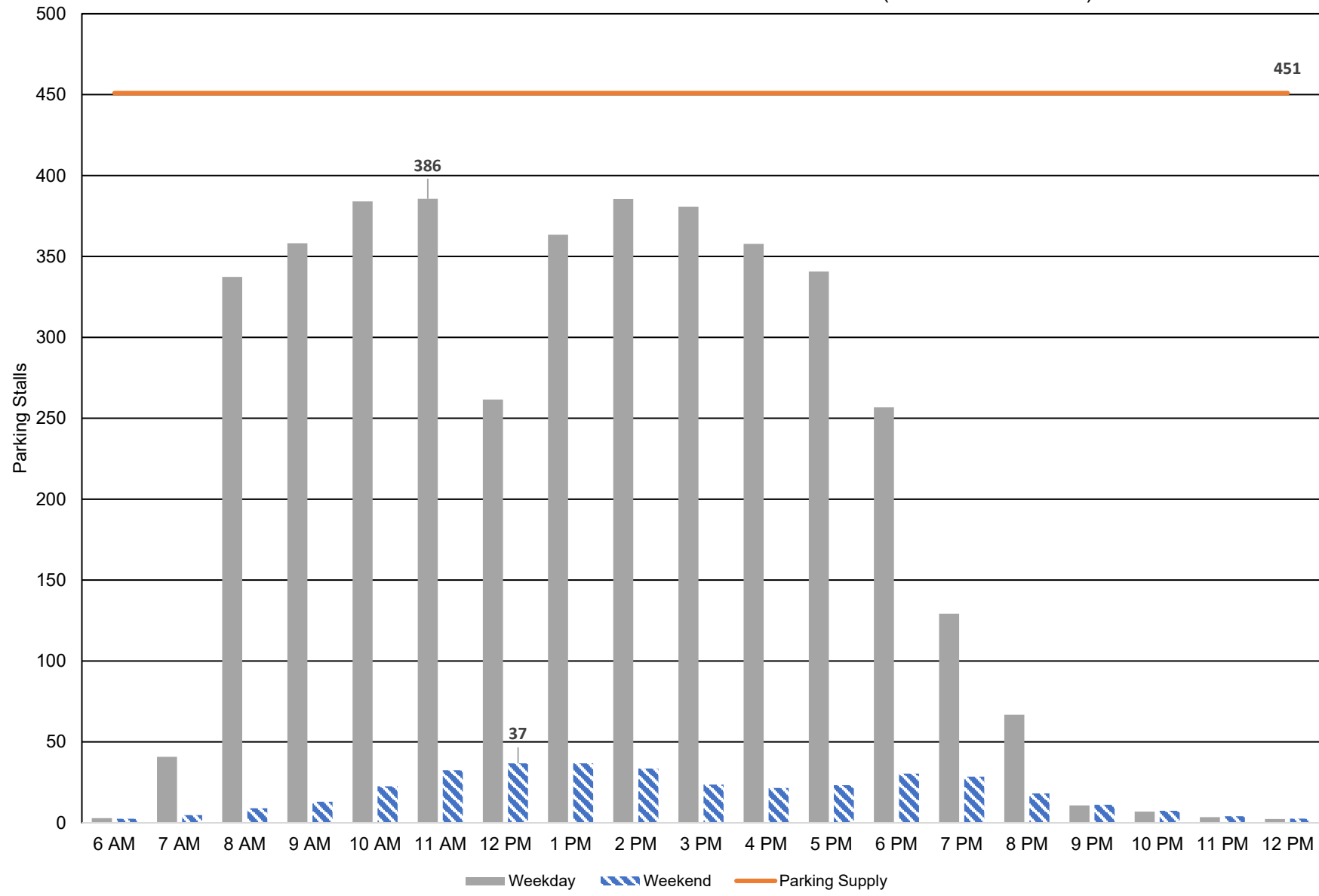


FIGURE 2
PEAK MONTH PARKING DEMAND BY HOUR BY LAND USE (WEEKDAY ONLY)
656 S. SAN VICENTE MEDICAL OFFICE PROJECT REFINED PROJECT (20% MEDICAL LAB)

